

TOWN OF LEDGEVIEW

Monroe Road Neighborhood Center District

PATTERN BOOK



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Mead
& Hunt

February 2018

acknowledgements

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introduction

Neighborhood Center Development

In 2007 the Town developed a Neighborhood Center District (NCD) section of the zoning code, and added NCDs to both the future land use (as Planned Mixed Use) and zoning maps. NCDs range in size (+/-10 to 40 acres) and are intended to be strategically located within walking distance of residential neighborhoods. While they encourage a mix of uses to serve immediate neighborhoods much like traditional downtowns, they may also contain regional attractions to serve a larger town and County population.

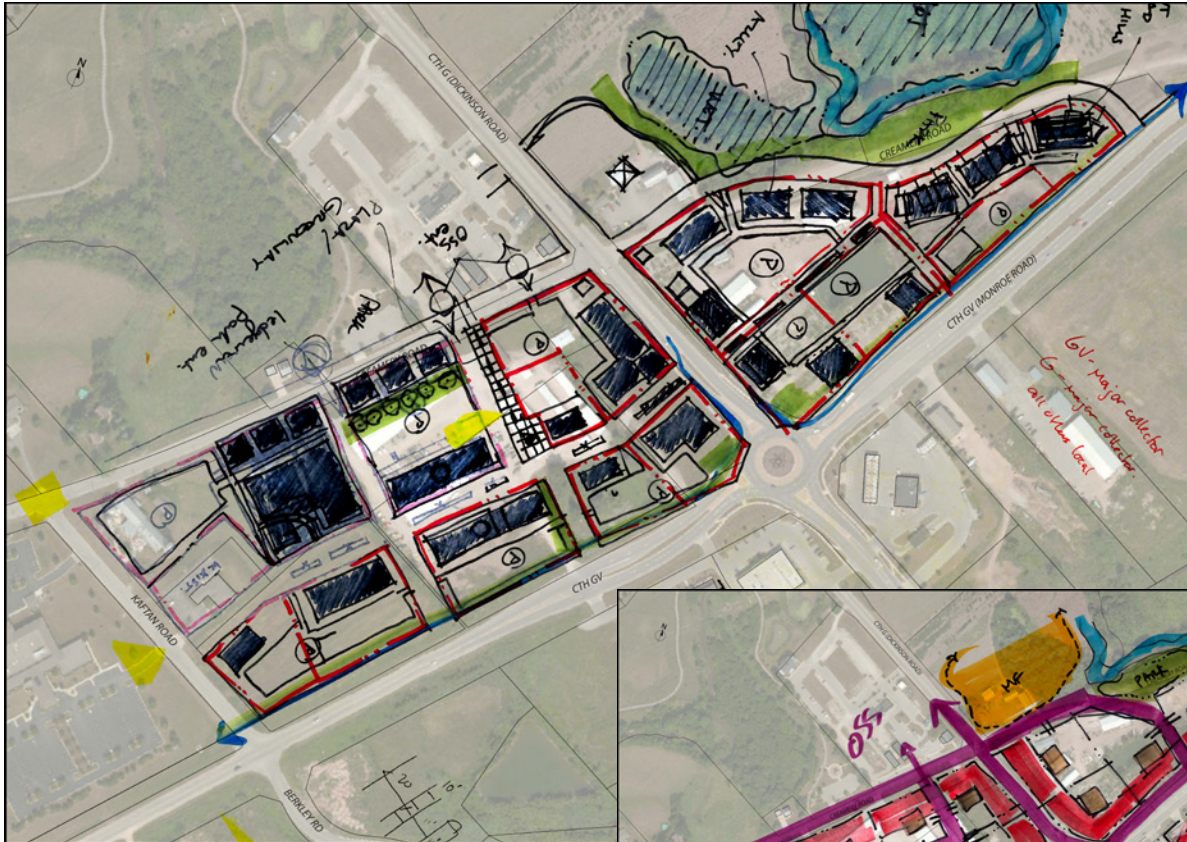
The Town's adopted "Smart Growth" Comprehensive Plan emphasize characteristics that can help make any neighborhood walkable, livable, and varied. To make neighborhoods more livable, it is recommended that NCDs be developed at strategic locations. The neighborhood centers should be strategically located within walking distance of residential uses. These centers should be located at a crossroads, encourage mixed uses, and provide goods and services geared toward the immediate neighborhood rather than the region as a whole. The neighborhood centers should be designed to reinforce the positive identity, character, comfort, and convenience of their surrounding neighborhoods, and access for pedestrians and bicyclists must be a priority. The CTH GV/Dickinson Road area—a major intersections for the Town—has been identified as a future neighborhood center.

The Plan also recommends that the Town should encourage community design elements, such as streetscaping, flags, banners, seasonal decorations, and signage controls, to aesthetically integrate individual land use areas.

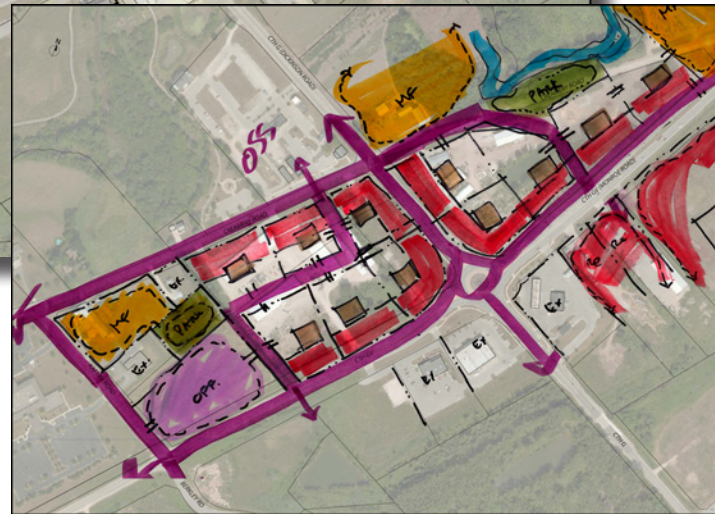


purpose & intent

Master Plan Concept Development Illustrations



Alternative Master Plan Concept



Master Plan

The Monroe Road NCD is the development of a vibrant, sustainable mixed-use area where residents and visitors can live, work, shop, dine, be entertained, enjoy community and cultural events, and contribute to the economic vitality of the community. The Master Plan provides an urban framework for the creation of a downtown setting within Ledgeview.

Purpose and Intent

The Pattern Book is used to regulate detailed aspects of the Master Plan. The principal intent of these design guidelines is to facilitate the implementation of the master plan and ensure that the Plan is implemented in the manner envisioned by the Town of Ledgeview.

The Pattern Book is based upon the Master Plan and serves to locate specific uses. Furthermore, it establishes the basic framework for the town center by regulating street locations, street types, building types, setbacks and “build-to” lines, parking locations, service locations, and other elements necessary for the successful implementation of the master plan.

The Pattern Book also regulates the architectural massing, building types, heights, facades, styles, materials, and details. It establishes the basic design guidelines that will ensure that the architecture of the individual buildings within the town center will all be in keeping with the overall vision. Also described herein are guidelines for open space, streets, parking, and service functions.

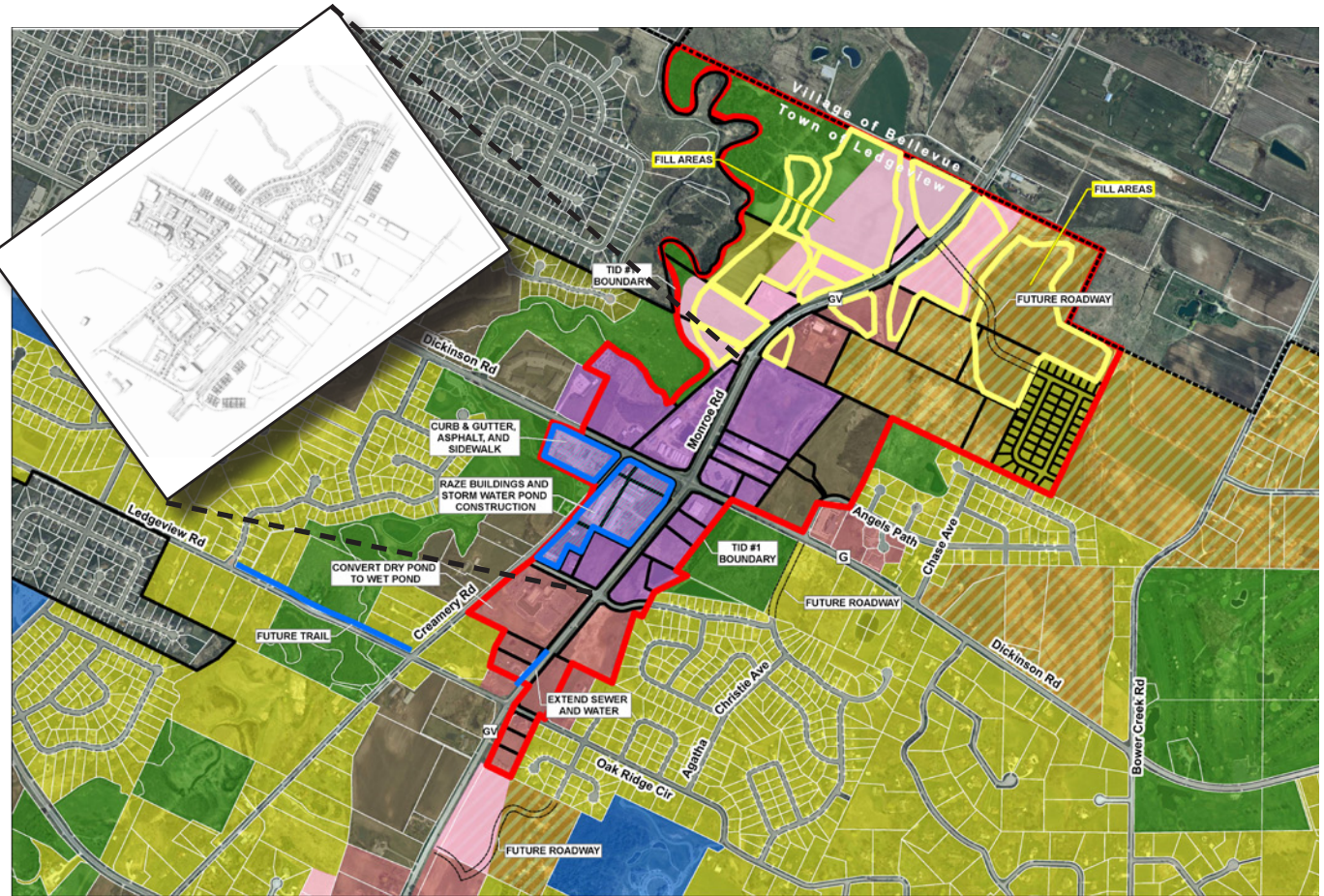
tax increment district #1

Tax Increment District #1

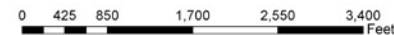
Tax Incremental District Number 1 (TID No. 1) is comprised of approximately 380 acres of land that creates the gateway for the northern entry into the Town. TID No. 1 is established as a “mixed-use” TID and will primarily contain commercial uses and residential, with a focus on commercial/retail/hospitality with office and residential support uses. One restriction within mixed use TIDs is that newly-platted residential uses cannot consist of more than 35% of the entire area of the real property within the TID.

The Town is fortunate in that it has not yet approved a significant amount of strip-type development. Strip commercial development has a tendency to be completely automobile focused in terms of the design and appearance of the shopping center. Typical of strip shopping centers is a sterile strip commercial building with significant setbacks from the road to enable large expanses of uninterrupted parking lots. Rather than developing a monotonous continuous strip of commercial development adjacent to either CTH G or CTH GV, use of Tax incremental Financing (TIF) will enable the Town to install pedestrian facilities and other public amenities that private development is often reluctant to construct.

Use of TIF will enable the Town to incentivize the development of pedestrian-scale commercial, residential, recreational, and institutional uses to recreate a sense of “place” for the Town. The area should include a number of small-scale commercial retail and service shops catering to both local residents and surrounding communities. The Town can require high-quality architectural and site design elements to



MAP 4: PROPOSED PUBLIC IMPROVEMENTS & USES



market demographics



One of the Fastest Growing & Highest Income Communities in the Green Bay Market

Trade Area Statistics

The Neighborhood Center District serves a market including Ledgeview, De Pere, and towns to the south. It has amazing potential to draw from a much broader area to its unique shoppes and restaurants.



The emerging regional business area includes several large retail businesses, such as Costco and Target. It also includes a WisDOT park and ride drawing many regional users. The planned extension of CTH GV (Monroe Rd) to the south, and associated bridge crossing will make Ledgeview Neighborhood Center District a prime location.

Targeted Opportunities:

- Specialty Food Stores
- Wine, beer, and spirit stores
- Luxury home furnishing stores
- Boutique clothing and accessory shoppes
- Jewelry shoppes
- Craft, novelty, and souvenir stores
- Banking and small office
- Personal Services
- Fitness and healthcare services

ensure a unifying theme for the town center.

Smart growth focuses on creating better access to goods and services and preserving a sense of place. TID No. 1 will enable the Town to spearhead the mixed-use development of this “gateway” area by providing common parking areas, provide safer vehicular and pedestrian movement in the intersection area, and continue to upgrade the visual image to of the community. The TID can further cement the District as an area for regional gathering, recreation, and commerce to create tax base and jobs.

To further the philosophy of “smart growth,” infill development will occur on underutilized parcels. These are properties with environmental constraints such as wetlands, floodplain issues, or in need of hazardous materials remediation. Concentrating on developing underutilized parcels will bring retail and office employment and housing to an underperforming area of the community. TID No. 1 will further the Town’s proactive efforts to improve land, increase property values, and to create job opportunities.

TID No. 1 will enable the Town to make infrastructure improvements that will provide safe bicycle and pedestrian facilities, while allowing for automobile traffic. This is critical to the success of the area.

Creation of TID No. 1 is necessary to fund the proposed public improvements and attract development to the heart of Ledgeview to accomplish the Town’s identified economic development goals. The TID will remove the burden of funding from the individual taxpayer and place the burden on the individual projects, the major direct benefactors of the improvements.

One Stop Shop for All Your Business Development Needs

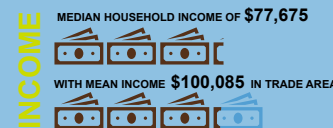
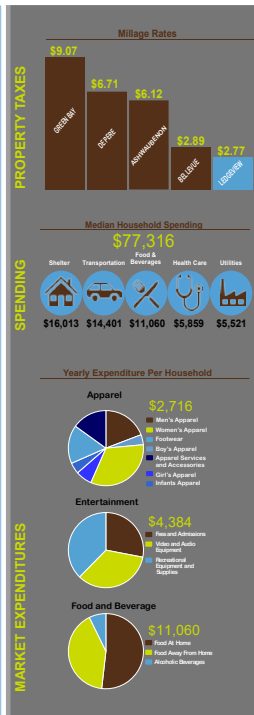
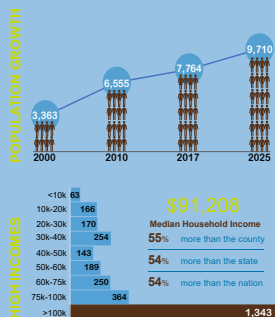
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Ledgeview Economic Snapshot

Ledgeview has very favorable economic conditions for starting or growing a business. Couple this with the ease of doing business in Ledgeview and there is no doubt you'll want to Grow With Us.



land use pattern

Land Use

The location of land uses is important to ensure that a vibrant mix of uses, that support each other, is achieved. The land use diagram at right illustrates a desired mix of land uses within the Town Center.

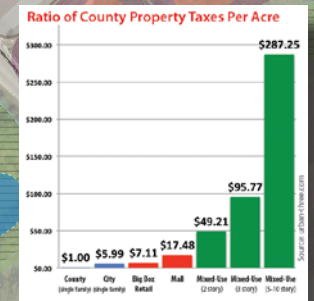
Especially important are the civic components which have been grouped together along the central boulevard. These elements are critical Town owned and maintained infrastructure consisting of a multi-use path, wide sidewalk / linear plaza, and market space. By locating these at the physical center, these civic components will be lined with private commercial, office, and residential users, balancing public and private investment.

Commercial uses are predominantly located between the central boulevard and Monroe Road. This serves two purposes. First, to create a strong urban street wall along the central boulevard and two to remain visible from Monroe Road.

Land Use Program

Ground Floor Square Footages

Commercial:	+/- 95,000 SF (20%)
Office	+/- 55,000 SF (12%)
Mixed-Use:	+/- 30,000 SF (7%)
Multi-Family Residential:	+/- 95,000 SF (20%)
Senior Residential:	+/- 23,000 SF (5%)
Parks and Greenways:	+/- 110,000 SF (24%)
Civic:	+/- 56,000 SF (12%)
TOTAL:	+/- 464,000 SF



land use character images

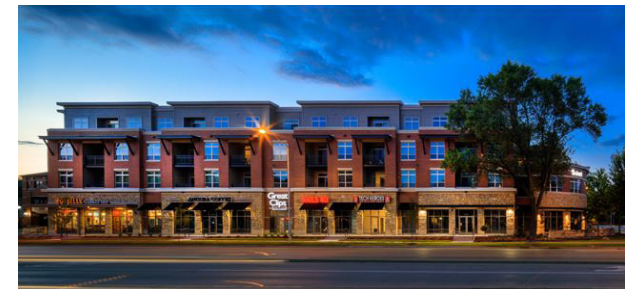
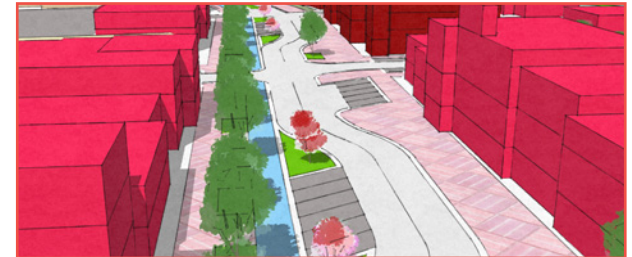
Multi-Family Residential



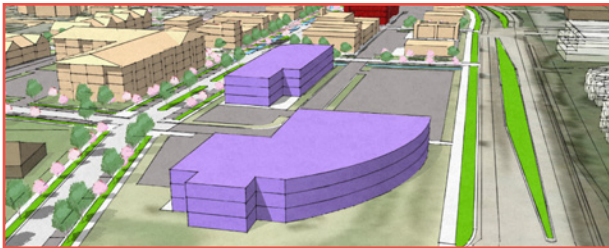
Senior & 4-Family Housing



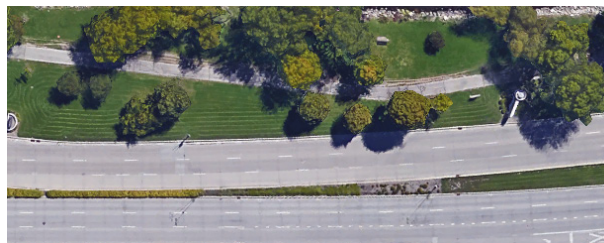
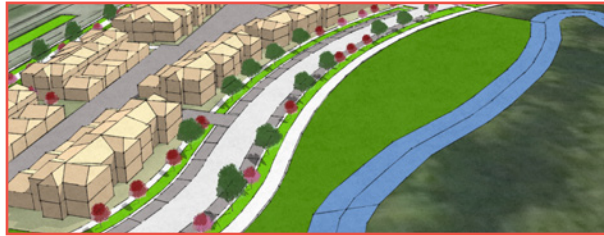
Commercial / Mixed Use



Office



Parks & Greenway



Public Space



alternative master plan & land use pattern

Alternative Master Plan Concept

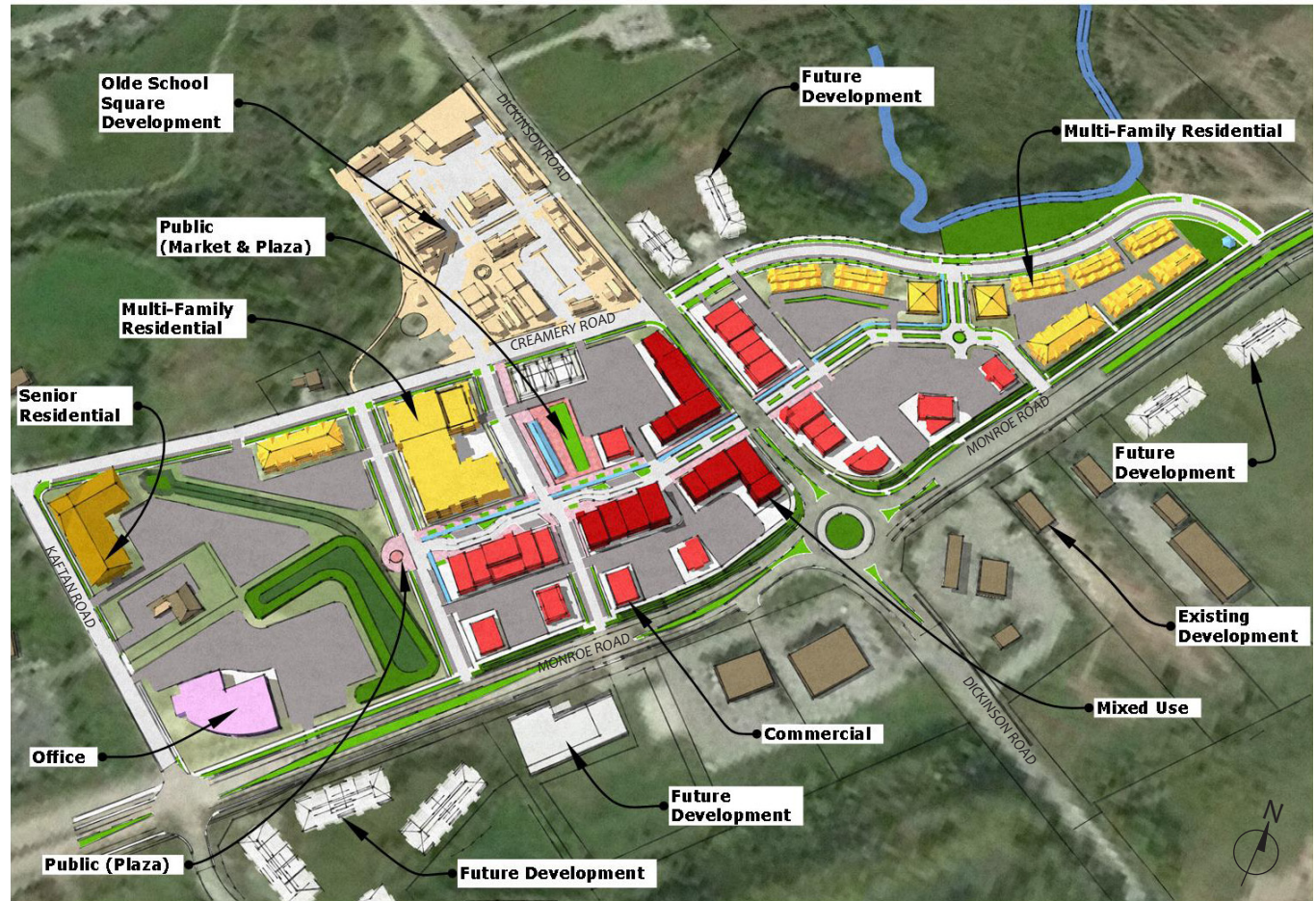
The alternative concept illustrates the influence of a large apartment complex with several hundred unit. This building typology would add significant density to the planning area, which is desirable. Strengthening the residential density can afford more demand for commercial services and support night-time activity.

Additional public space was added to complement the additional density and better connect the proposed main boulevard to Olde School Square.

In addition to the residential structure, this alternative considers reducing stand-alone office space in favor of a mixed-use structure. Office, as a use, is desirable to bring business patrons to the site daily, but the amount of stand-alone office buildings may be less in the planning area compared to other business park areas in Ledgeview; namely along I-43 and Swan Road.

Finally, this alternative considers a large wetland area and navigable stream under the purview of the WDNR. This area has a large influence on buildable square footage but can be a natural asset in terms of views and passive recreation.

It should be noted, this alternative is only presented on this and the following page (page 13). Other images and figures throughout the document reflect the Master Plan Concept found on the previous pages.



Multi-Family Residential



Senior Residential



Public Space



Office



Alternative Land Use Program

Ground Floor Square Footages

Commercial	+/- 70,000 SF (15%)
Mixed-Use:	+/- 50,000 SF (11%)
Office:	+/- 20,500 SF (4%)
Multi-Family Residential:	+/- 127,500 SF (28%)
Senior Residential:	+/- 23,000 SF (5%)
Parks and Greenways:	+/- 110,000 SF (24%)
Civic:	+/- 60,000 SF (13%)
TOTAL:	+/- 461,000 SF

public & gathering spaces

Spaces designated for the public are critical components to a pedestrian friendly environment. These types of spaces provide areas for people to congregate, people watch, and relax. Multiple public spaces should be designed within the NCD, located at different points of intersection. Making these spaces widely available and accessible to all users is the goal.

1. Plaza Walk

The Plaza Walk is meant to be an open air plaza with a purpose - to connect the central boulevard to Olde School Square. Several textures should be used to identify this space as unique from other pedestrian areas. The space may also house sculptures or other public art, host food trucks or vendors, or simply have movable tables and chairs for public use. It's placement is intentionally offset to increase visibility both from Olde School Square and Monroe Road.



2. Market Arcade

The Market Arcade is meant as a Town owned facility that can be used for public markets, events, or festivals, or simply as a gathering place within the NCD along the central boulevard. It is designed to hold vendors or simply be used to denote the direct connection to Olde School Square (OSS) to the west.



3. Central Boulevard

The central boulevard is a new “Main Street” for Ledgeview. This street brings together residents, visitors, and business owners in a highly interactive environment. By placing buildings along the central boulevard, an enclosed, pedestrian environment can be created. Monroe Road provides access and visibility for development, so allowing parking along it is highly beneficial for all parties.



4. East River Park

East River Park is created by realigning Creamery Road away from the East River and it's associated wetlands and floodplain. A multi-use path brings multi-modal access and the placement of multi-family residential apartments or condominiums along Creamery Road will define the streetwall and provide eyes on the park both day and night.



5. Point Park & Monroe Road Greenway

Point Park is meant to be a gateway on the point of Creamery Road and Monroe Road that identifies one entrance into the NCD from the north. A simple marker, such as a gazebo, would identify the public space and define it's availability for public use. As one moves along Monroe Road, the greenway provides both aesthetic and function as it can be used for stormwater retention as well as recreational use along side the multi-use trail.



build-out & frontage

Buildings

Buildings should be oriented with entrances and transparent facades facing the public sidewalk and street. Buildings should line the central boulevard to create a continuous edge condition. Distinction between ground floor and upper floor levels should be clearly articulated (e.g. through a change in material or setback distances). Commercial first stories should have a 16' minimum floor to floor height. Blank walls (e.g. grocery stores or small box stores) should be lined with street-fronting, compatible uses.

Height

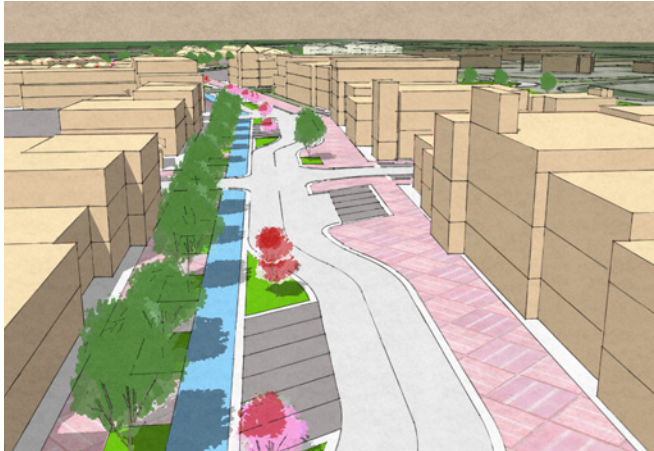
The diagram to the right shows the building heights recommended for the NCD. Building height is a critical factor in traditional town centers because the roofline aids in defining the street and the pedestrian environment. The upper floors of buildings that are too tall are lost in context because people on the ground cannot see or interact with them. Similarly, buildings that are too short leave a void above which detracts from the streetwall. Thus, building heights also contribute significantly to the character of a place. Building heights, and thus the amount of facade that contains articulation and design, should range from 2 (minimum 25 feet) to 5 stories.

One Story Buildings

Stand alone one story buildings may be designed with a tall first floor and a false second story. Minimum building height for one story buildings is 20 feet.



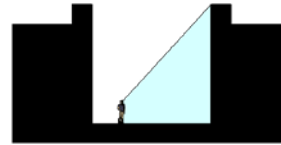
streetscape



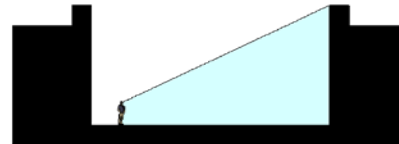
The urban streetwall on the central boulevard shown above has a 4:1 ratio mid-block and a 3:1 ratio at the intersection with Dickinson Road.



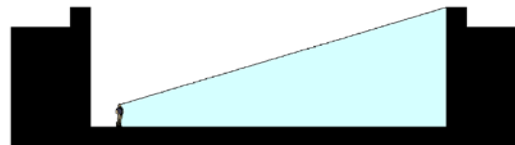
The redevelopment of Creamery Road provides a residential streetwall against the relief of East River Park, which helps define the character of the street, as well as the park space.



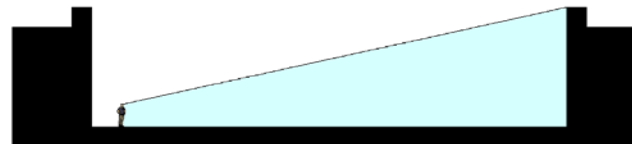
1:1 Ratio. The strong sense of enclosure can aid in calming traffic.



2:1 Ratio. Minimum recommended by American Planning Association and within the Development area.



3:1 Ratio. Maximum ratio recommended within the Development Area.



4:1 Ratio. Not recommended in the Development Area.

Building Enclosure

Building enclosure, or the ratio of building separation to building height, defines urban space. It is important for achieving a certain aesthetic, level of comfort, and character. Building ratios of 1:1 to 4:1 generally require narrower streets when building height is limited to 5 stories. Ratios higher than 4:1 may lose a sense of enclosure and should be avoided. The American Planning Association recommends a minimum building separation to building height ratio of 2:1.

Urban Streetwall

The streetwall is the composition of building placement and orientation of their primary facades to enclose and define a relationship to the adjacent pedestrian realm, typically the sidewalk. Having a continuous streetwall creates an edge and a sense of enclosure which, in Ledgeview, will distinguish the NCD from other commercial areas. This should establish a sense of place which is more comfortable for pedestrians. For instance, because a building is fronting a street and has a highly transparent facade, a pedestrian has the ability to see action inside from the sidewalk, creating interest and engagement.

The central boulevard should have a strong streetwall with a ratio no less than 3:1. Few gaps should exist between buildings except for at intersecting streets. In the Master Plan, a void is left in the streetwall at the Market Arcade, which acts as a meeting point for users. This void provides relief in the enclosed streetwall and directs attention to OSS which encourages further exploration of its connection to the central boulevard.

architecture & character

The Town of Ledgeview encourages the creation of unique aspects to each building. Architectural projections, trim, recesses, decorations, various styles, and other features bring substance and distinctness to the community. A mix of quality and aesthetically-pleasing materials should also be used for any new construction taking place in the NCD. Bland architecture with little or no urban character and materials of low-quality should not be used.

The NCD should provide unique architectural components to distinguish the area from other downtowns and shopping areas. The architectural style may be consistent with that of adjacent Olde School Square. OSS currently offers some of the most unique buildings in northeast Wisconsin which showcase 19th century architectural styles. OSS architecture is a combination of the Federal, Greek Revival, and Colonial Revival styles. Revival style architecture is the incorporation or echoing of previous design styles in a new design. Elements of other period styles such as Georgian or Italianate may also be appropriate in particular applications.



Frontage and Façades



Strong Corner Elements



Frontage

Buildings are to be used to create a visual edge along public streets. Building faces must be built to the “Build to Line”, or setback. Building lines must have a minimum of 60% of their frontage occupied by the building face. Building lines bordering plazas must have 95% of their frontage occupied by building face. For retail uses, the minimum percentage of surface that is glazed is 70% of the Building Face. No reflective or tinted glass is permitted on the ground floor.

Façades

Sections shall vary in length and in height. To avoid a monolithic appearance, facades shall have varying designs and break down buildings into smaller sections. The use of awnings and signage is an effective way to achieve differentiation. Each façade section may also vary in type, material and color.

Windows

Transparency is especially important on corners. Windows shall be clear glass. No reflective or tinted glass is permitted. Window sills and lintels, when present, shall project a dimension consistent with the specified architectural style. Glass block is not permitted. Window sills shall project from the building face. Maximum height of sill above the sidewalk is 3 feet.

streetscape details

Pedestrian zones, or the public space between the parking or travel lane and a building, is to be furnished with usable amenities. It's important to note that some elements of the pedestrian zone may be private or privately provided and some may be public and publicly provided. Elements may also be layered in overtime (i.e. phasing) building on the street character, rather than trying to define it all at once.



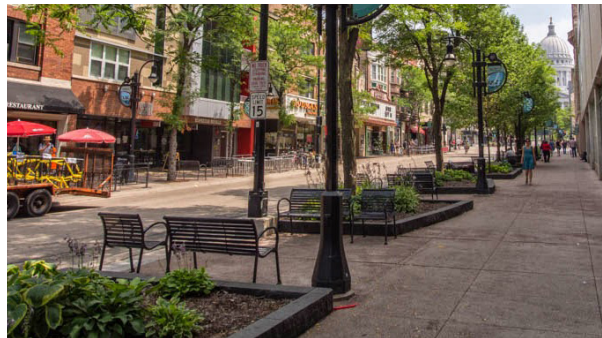
Seating

Benches or seat walls should be provided in public spaces and along public ways. Permanent seating (benches) should be placed to face each other, creating engaging spaces. Benches, like light poles and other street elements, don't have to be overly decorative. Rather, the use of color can define the character.

Adjustable Space

Moveable chairs and tables shall be utilized in the pedestrian zone. Moveable chairs and tables allow

Seating



Adjustable Space



Shared Streets



Lighting



people to make a place their own and to use the space rather than simply passing through it. And, activation supports commercial uses. Movable chairs are particularly important for seniors who may need to adjust the location of seating to their needs. Urbanism is inherently cluttered. But that doesn't mean disorganized. As such, trash receptacles should also be provided in the pedestrian zone.

Shared Streets

Fast big things and slow small things do not mix well. However, when they are mixed, the outcome is that streets become a different kind of public space, where mobility means interacting with one's surroundings. Streets typically use a vertical curb. But a lack of curbs or use of a mountable curb can encourage the use shared streets. It can also create a place for temporary uses such as food carts and food trucks. Decorative pavers can delineate various rights-of-way, but the lack of a physical curb creates a space where users must engage. Physically leveling the plain creates a cognitive response that multiple users are utilizing a space, so extra care should be taken to look for those users.

Lighting

The best lighting comes from storefronts shining on sidewalks. As such, front setbacks must be minimal. Other street lighting is typically oriented for vehicles but multiple types of street lighting - for pedestrian and vehicles - is desired. Street lighting fixtures do not have to be ornamental. Rather, character can be derived from the use of colorful poles and fixtures.

street framework

Streets

Town streets are not just vehicular service ways, but create access and form the public realm. Streets are to be designed with the pedestrian in mind. Front doors of buildings greet the pedestrian, street furniture and programmable spaces provide activation, and street trees provide a shaded canopy.

Each street type reflects a unique character within the NCD. Streets vary from a more urban character to a more intimate informal character depending upon their use and location. Chapter 96 of the Ledgeview code should be consulted for street width requirements. The recommended dimensions are outlined below.

Major Collector (CTH GV and CTH G):

12-foot travel lane, no parking.

Local Road:

Creamery Road:

10-foot travel lane, 8-10 foot terrace, 6-foot sidewalk or 10-foot trail (north)

Central Boulevard:

10-foot travel lane, 9-foot parking

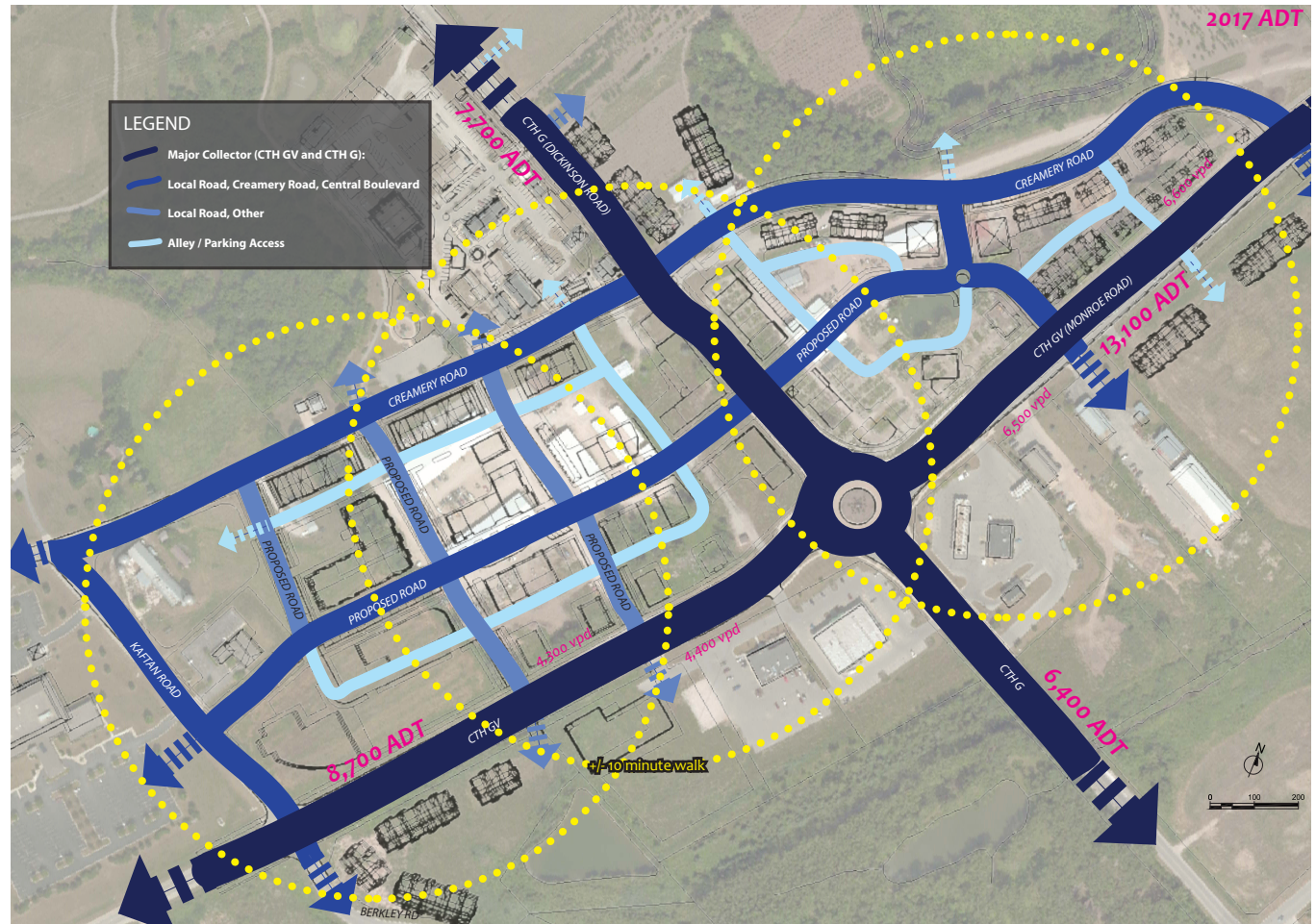
- With 10-foot boulevard: 20-foot sidewalk, no terrace
- With no boulevard: 20-foot sidewalk, 6-foot terrace

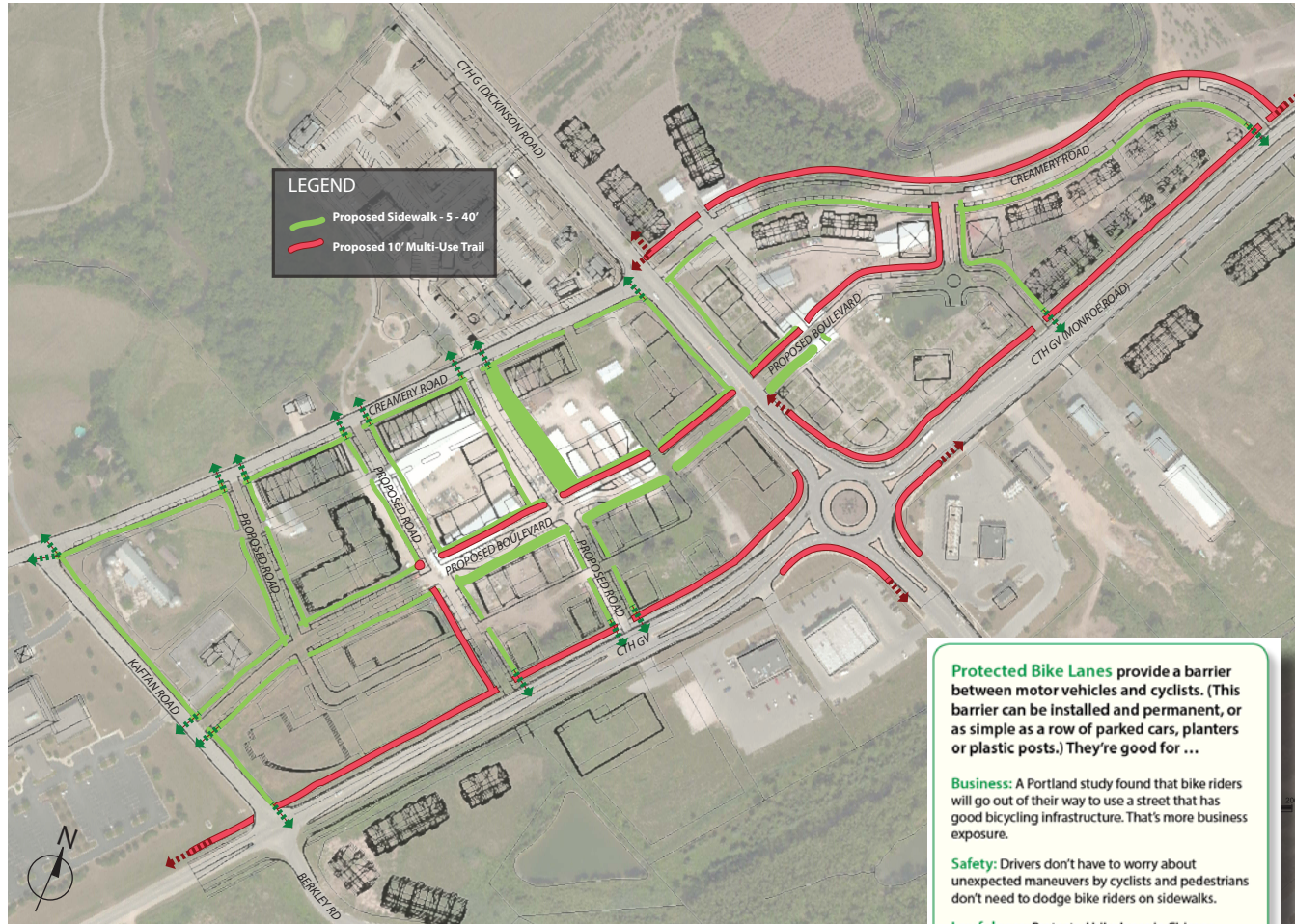
Other:

10-foot travel lane, 6-foot terrace, 6-foot sidewalk

Alley / Parking Access:

20-24-foot curb to curb





Pedestrian Circulation

Pedestrians must be exposed to all public streets via a sidewalk or multi-use path. In particular, smaller blocks allow pedestrians more options for access throughout the planning area.

Bicycle Access

Multi-use paths are proposed for shared use between pedestrians and bicycles. One path, which currently exists or is programmed for construction, runs along Monroe Road. Another path is proposed along the north portion of Creamery Road. This path will convert to an on-street accommodation south of CTH G.

Bikers are the fastest growing part of the mobility group - and they buy stuff. Bicycle accommodations should be provided along the central boulevard to provide direct bicycle access to storefronts, offices, and other uses.



Protected Bike Lanes provide a barrier between motor vehicles and cyclists. (This barrier can be installed and permanent, or as simple as a row of parked cars, planters or plastic posts.) They're good for ...

Business: A Portland study found that bike riders will go out of their way to use a street that has good bicycling infrastructure. That's more business exposure.

Safety: Drivers don't have to worry about unexpected maneuvers by cyclists and pedestrians don't need to dodge bike riders on sidewalks.

Lawfulness: Protected bike lanes in Chicago resulted in a more than 150 percent increase in the number of bike riders obeying traffic lights.

Everyone: Bicycles don't pollute, they cause less wear and tear on roadways than cars do, they help people stay healthy!

Source: Adapted from the Transitized.com infographic "Why Build Protected Bike Lanes?"

parking & the right-of-way

Parking Locations

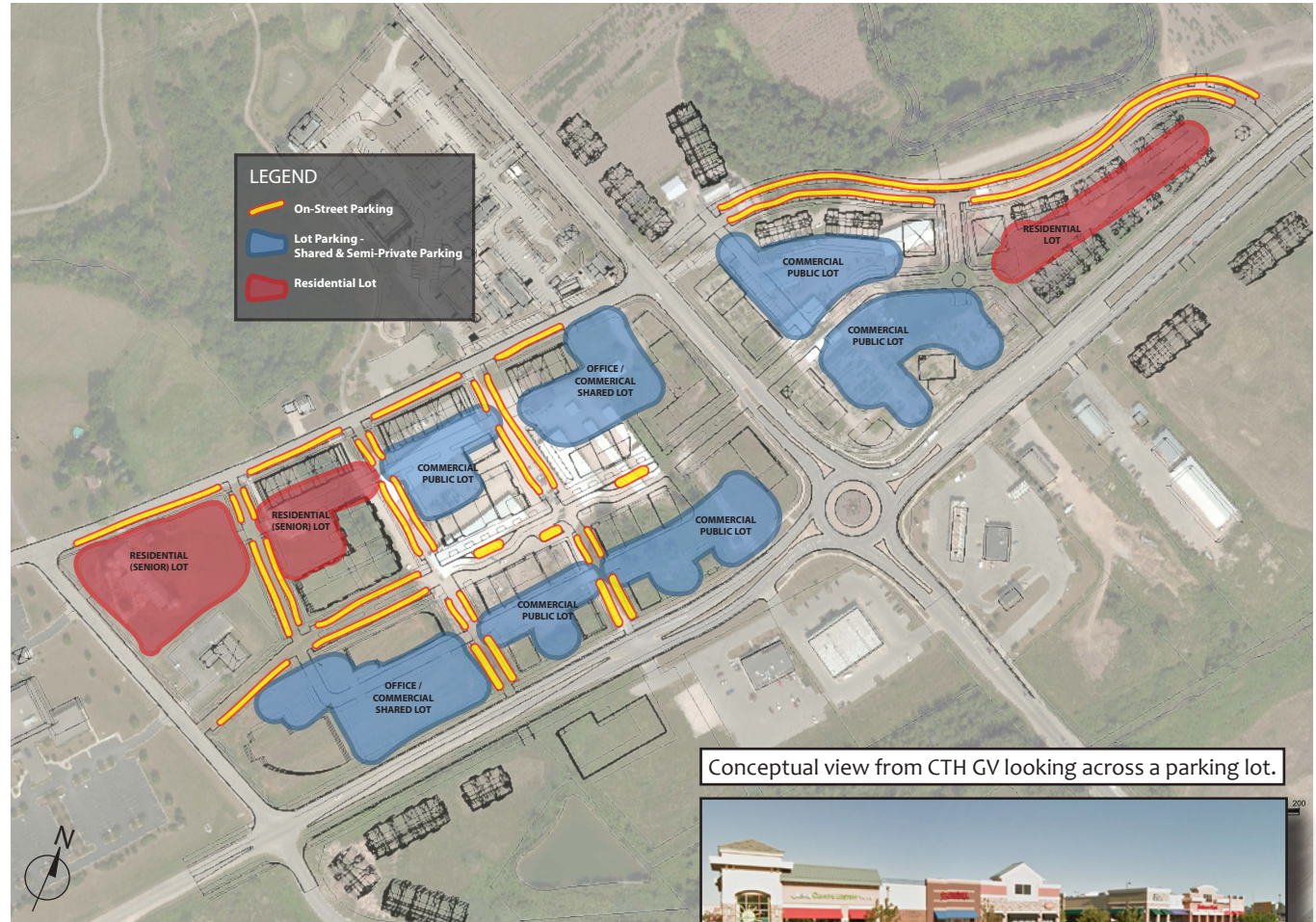
Parking lots and large setbacks are some of the most common attributes of auto-oriented development. In successful traditional urbanism, parking is available both in front and in the rear of buildings. On-street parking in the NCD is necessary because it provides teaser (i.e. short-term) parking for storefronts and provides a buffer for sidewalk users and uses.

The majority of parking in the NCD should be located in the rear of commercial buildings, in this case, along Monroe Road. By placing the parking in the rear, a streetwall can be created, providing a more pedestrian friendly atmosphere. Parking should also be provided along the central boulevard.

It is desired that areas of parking provided along Monroe Road be infilled with development overtime. While Monroe Road provides visibility for users, it is a major collector primarily intended for mobility rather than access to development. This means the amount of curb cuts will likely be limited.

Shared Parking

Shared parking makes spaces publicly accessible rather than reserved for a particular tenant or property owner. Parking demands operate on a peak and off-peak schedule depending on their associated land use. Complementary uses such as office - which is generally empty in the evenings and on weekends - and residential offers an opportunity to share parking rather than increasing demand.



Conceptual view from CTH GV looking across a parking lot.



Traditional Boulevard



Shared Street



Central Angle Parking



The NCD provides opportunity for shared parking with OSS, particularly on weekends. Additionally, the emergence of the autonomous vehicle (AV) has the potential to shape the physical setting and potentially reduce the need for parking. For example, more cars may circle a destination rather than park.

Parking on the Central Boulevard

The portion of a road typically reserved for parking or loading of vehicles may play other roles as well. These areas may provide a variety of streetscape and pedestrian amenities including landscaping, stormwater treatment, bicycle parking or seating and represent a significant untapped resource to extend the pedestrian realm and quality of the pedestrian experience.

Parking along the central boulevard should be designed in a way to provide more than simply parking for vehicles. Parallel parking can provide a buffer between the travel lane and large sidewalks and patio areas. This also provides opportunities for bulb-outs at corners, mid-block crossings, and a landscaped boulevard - all amenities designed to encourage or support pedestrian comfort and movement.

Angle parking also provides these amenities, but can further slow traffic and requires slightly less space, meaning more parking may be available.

Attractive materials such as pavers or pavement marking can distinguish the parking lane from other lanes. Specifically, the use of permeable pavers, which can also help reduce stormwater runoff.



lots & blocks

Block Size

Smaller blocks are more walkable and accessible and are the preferred block type in the NCD. When blocks are smaller, pedestrians are more apt to mobilize on foot for a second or third destination. This, in turn, reduced automobile trips and congestion. While over 95% of households in Ledgeview have a vehicle, and this has traditionally been the method of mobility in the Town, the NCD is intended to prioritize other modes of travel. Furthermore, the best place to retail is the corner. When there are large blocks, there are fewer corners.

The NCD block structure was designed by referencing communities surrounding Ledgeview. In De Pere, the traditional neighborhoods block structure can be seen. Small blocks allow multiple routes for multiple modes. In Green Bay, the larger blocks are associated with auto-oriented strip commercial development - something existing along CTH GV in Ledgeview today. In Bellevue, a big box development leaves little opportunity for arrival but by car. As the NCD develops, the area around it will too develop, creating density and supporting the smaller blocks.

The Grid System

An effective way of creating and maintaining a highly accessible area is through the use of the grid system. This system is practical in nature and makes it easy to approach an area from any direction, and establishing connections to area of existing development. It is also very pedestrian friendly because of the relative ease one has in orienting themselves when in the grid.

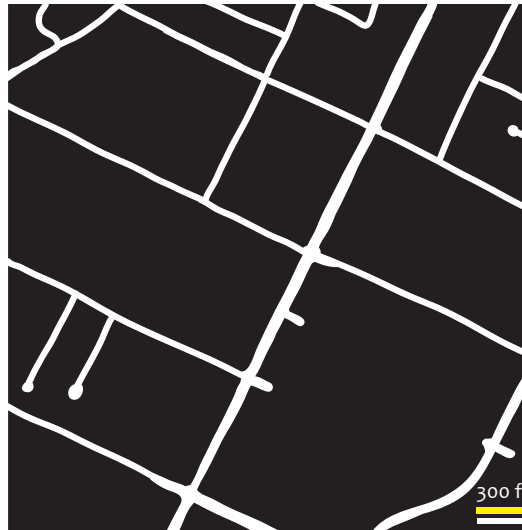
Ledgeview NCD



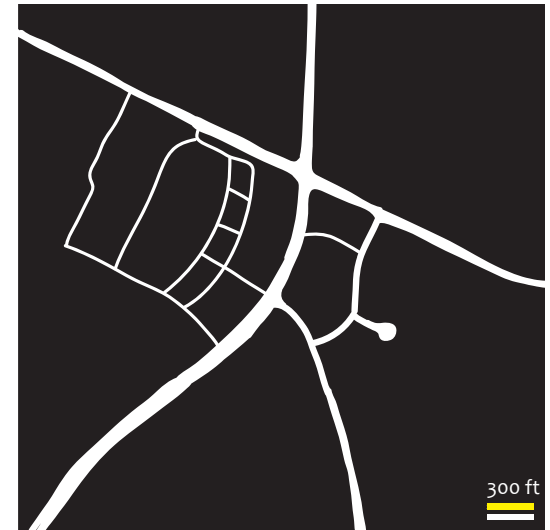
Downtown De Pere

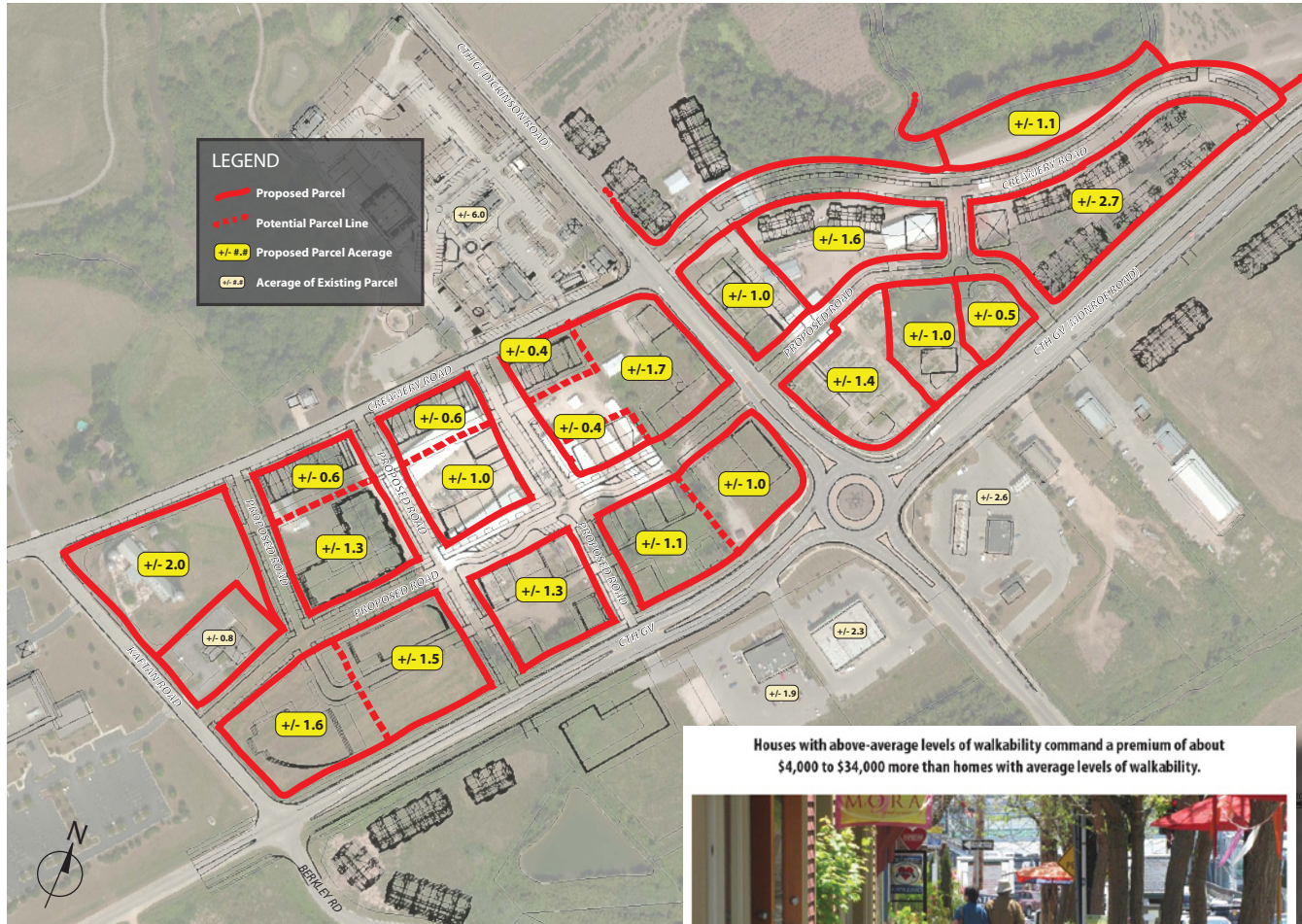


Oneida Street at WIS 172, Green Bay



CTH GV at Allouez Avenue, Bellevue





Breaking down the Superblocks

A superblock is a large plot of land that occupies a larger than typical block in the context of an otherwise uniform grid system. Superblocks disrupt the street grid and limit accessibility to areas surrounding it. In traditional neighborhoods, large public uses such as schools often occupy superblocks.

While uses located on a superblock may serve the community and surrounding commercial area, they also have the ability to hinder access, visibility, and circulation in the surrounding blocks. Existing superblocks should be broken down into smaller, traditional neighborhood scale blocks by introducing a hierarchical street network pattern. Transportation management on County roads may limit the number of access points (driveways or new roads) intersecting both CTH GV and CTH G. But introducing a grid pattern will greatly improve the accessibility and connectivity to and through the site and the surrounding areas. New superblocks should be discouraged from forming.

Houses with above-average levels of walkability command a premium of about \$4,000 to \$34,000 more than homes with average levels of walkability.



This block in Kingston, Wash., is located between a strip mall and a street. Prior to the shops and sidewalk being built, the space was simply a parking lot. Now there's retail on an active streetfront and still adequate parking.

AARP
Real Possibilities

Walkable and Livable
Communities Institute

Parcel Acreage

Acreages of parcels dedicated to each land use is presented below.

	Acreages
Commercial & Mixed-Use:	+/- 9.1 AC
Office:	+/- 4.8 AC
Multi-Family Residential:	+/- 5.9 AC
Senior Residential:	+/- 3.3 AC
Parks and Greenways:	+/- 4.2 AC
TOTAL:	+/- 27.3 AC

services

Drive-thru's

Consideration should be given to the location and orientation of drive-thru uses and their parking. Drive-thru's don't typically require the same amount of parking as traditional sit-down restaurants, so minimum parking may be considered rather than maximum parking. Similarly, shared parking may be used for restaurant uses or to meet requirements for drive-thru's. Drive-thru's can be designed with thru-lanes in parking lots and not out to Town roads, as shown in the example at right.

Furthermore, drive-thru's can be placed within a pass-through structure to minimize their visual impact on the pedestrian environment. Entrances and exits on different streets may also allow better site circulation as drive-thru users are somewhat separated from general parking lot users and or residents of a multi-use or adjacent building.

Service Areas & Refuse Collection

All trash and recycling service areas shall be enclosed so as not to be visible. Enclosures should match the architectural style of the principal structure with which they are associated.

Service Entrances

Service areas, ideally, are to be located to the interior of the block behind buildings. When loading cannot be located to the rear, service from the street side should be located within the building envelope and have closing doors. The service bay should be designed to be

Drive-Thru's



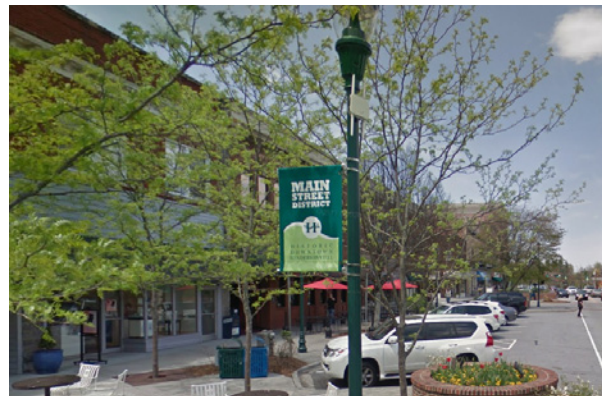
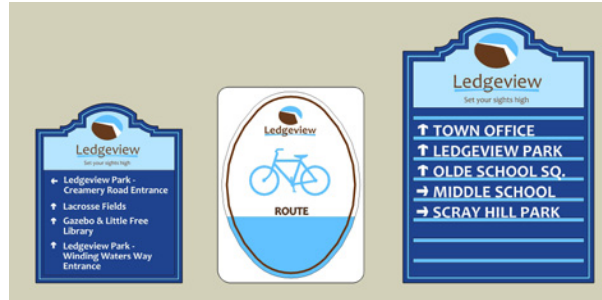
Service Areas & Refuse Collection



Building and Mechanical Equipment



Wayfinding & Signage



compatible with the primary building facade. Openings shall only extend for two stories - it is encouraged that habitable building be located above garage openings. It is also encouraged that entries shall have lanes divided by columns set at the building line.

Loading

Loading that does not occur within an enclosed garage area will be located on the street and signed as a loading zone. Loading should primarily occur on side streets.

Building and Mechanical Equipment

HVAC equipment, utility meters, satellite dishes, permanent grills, and other mechanical equipment should be located so as not to be visible from the street. They should be located to the interior of the block or on roofs and shall not be visible from the street. Mechanical equipment should not vent to the street side of the building.

Site Utilities

Site utilities structures such as transformers shall be located to the interior of blocks (behind buildings). Screening is desired.

Wayfinding & Signage

Signage is one of the most important wayfinding tools. The NCD should contain unique identification and wayfinding signage both for public amenities and individual businesses.

green infrastructure

Street Trees

Trees in urban settings are in a highly stressful environment. In order to maintain the viability of a tree canopy, tree pits should be used. Tree grates should be used as a decorative cover to tree pits. Street trees require sufficient space to mature, meaning their distance from buildings should accommodate a full-grown canopy. Minimum size for tree planting shall be at least 3 1/2" caliper. Trees should be spaced at approximately 30-foot on center when placed in a row and should use a random, or "salt and pepper" approach to type selection.

Paving

Crosswalks and the central plaza shall be some form of special paving. Special paving is defined as scored concrete (in a decorative pattern), stone pavers, brick pavers, or concrete unit pavers. Vehicular access points from public streets to parking areas may be composed of pavers to indicate parking areas. Paver selection and paving patterns shall be consistent.

On-street parking along the central boulevard shall be comprised of permeable pavers intended to improve stormwater quality.

Planters

Planters help to furnish the streetscape and provide barriers for errant vehicles between building frontages and streets. Any number of planters may be used, but each should contain vegetation planted for the season.

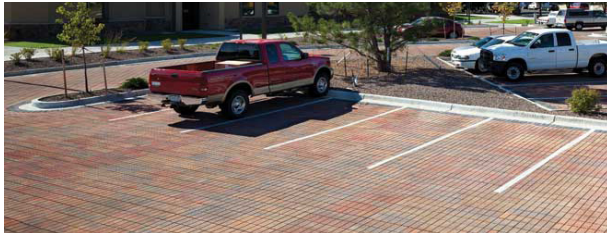
Street Trees



Planters



Paving



Bioswales and Flow-through Planters



Stormwater Management

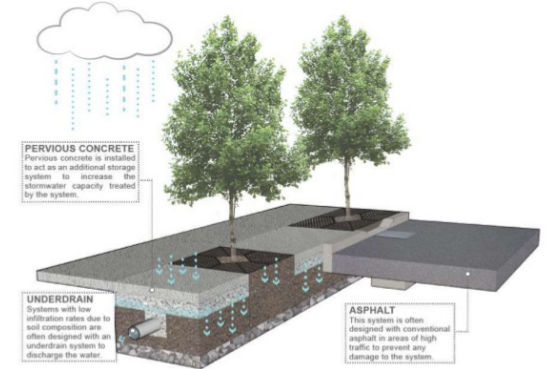
Conventional stormwater management infrastructure has been engineered to move the largest volume of water from a site as quickly as possible, collecting surface runoff in subsurface structures. Sustainable stormwater management captures water closer to the source, reducing combined sewer overflows, ponding, and roadway flooding. Sustainable stormwater management can prove less costly than upgrading large sub-grade pipe networks, and allows for flexible, modular installation.

Stormwater runoff is detained in facilities such as flow-through planters, pervious pavements, and bioswales. Sustainable stormwater systems reduce the amount of stress on municipal wastewater treatment facilities, and may reduce long-term costs. Unlike traditional infrastructure, green infrastructure can be incorporated into neighborhood parks and landscaping.

RUTGERS

Water Resources Program

Stormwater Tree Pit/Street Trees



glossary of terms

Bulbout: a limited curb extension that narrows a street to reduce the pedestrian crossing distance of a street.

Common Area: areas that are not owned by an individual owner of a residential unit but shared by all owners, either by a percentage interest or owned by the management organization. Common areas can include the lobby, hallways, parking areas, laundry rooms, gathering spaces, recreational facilities, clubhouses, community centers, parks and other outdoor open space, landscaping, fences, and all other jointly used space.

Condominium: a building or complex of buildings containing a number of individually buildings.

Cornice: An architectural detail that decorates a roofline.

Cupola: A small structure projecting above a roof that provides ventilation or is used as a lookout, especially with a hemispherical roof on a circular or polygonal drum.

Dormer: A small structure that projects from a sloping roof with a window in the down slope end; used to light an attic space and to provide headroom; may have a gabled, shed, or other shaped roof.

Double-loaded Street: a public street or space that has buildings on either side, facing the street or space.

Eave: The projection of a roof beyond the wall; most often used to refer to the edge and underside of a roof.

Forecourt: An open area in front of a large building.

Glazing: The clear or translucent material, usually glass, through which light passes into a building.

Glazing, Reflective: Window glass which has been coated on the outside with a transparent metallic coating to reflect a significant fraction of the light and radiant heat which strikes it.

Greenway: A linear park, alternative transportation route, or open space (conservation) area that provides passive recreational opportunities, pedestrian and/or bicycle paths, and/or the conservation of open spaces or natural areas.

Massing: The overall composition of the exterior of the major volumes of a building.

Proportion: The relationship of the size, shape, and location of one building element to all the other elements; each architectural style typically has its own rules of proportion.

Single-loaded Street: a public street or space that has development on only one side, facing the public street or space.

Streetscape: The space between the buildings lining the public ways that comprise various elements such as streets and alleys, sidewalks, monuments and public art, and small-scale features such as curbs, paving, street furniture, lighting, fences, walls, and landscaping.

Streetwall: A streetwall is the composition of building facades, building enclosure, and building relationship to adjacent sidewalks.

Urban Design: The arrangement and design of buildings, public spaces, transport systems, services, and amenities. The process of giving form, shape, and character to groups of buildings, to whole neighborhoods, and the city by ordering the elements into a network of streets, squares, and blocks. Urban design blends architecture, landscape architecture, and city planning together to make urban areas functional and attractive.

Terrace: the space between a sidewalk and street, commonly landscaped or used for tree planting.

Turret: A small, projecting tower at the corner of a building, or above the roof; typically circular or octagonal in plan.

adopting resolution

RESOLUTION # ZPC 03 -2018

RECOMMENDATION OF APPROVAL OF MONROE ROAD NEIGHBORHOOD CENTER DISTRICT

WHEREAS, The Town of Ledgeview has adopted the Neighborhood Center District as Article XX of Chapter 135: Zoning; and

WHEREAS, The purpose and intent of the Neighborhood Center District is to provide for a variety of pedestrian-oriented retail, office, lodging, residential, and civic land uses in the Town center area and encourage development and sustainability of a vibrant mixed-use area; and

WHEREAS, The Town of Ledgeview developed the Monroe Road Neighborhood Center District Pattern Book to serve as a reference in guiding and reviewing future development within the area zoned Neighborhood Center District, and

WHEREAS, The Town of Ledgeview has adopted Tax Incremental District #1 which includes the Monroe Road Neighborhood Center District, and

WHEREAS, Said Pattern Book sets the basic framework for the Monroe Road Neighborhood Center District by prioritizing block sizes, street location, street types, building types, setbacks and "build-to" lines, parking locations, and service locations necessary for the successful implementation of the Master Plan; and

WHEREAS, Said Pattern Book sets design guidelines for architectural massing, building types, heights, facades, styles, materials, details and other design elements necessary to ensure that the architecture of individual developments keeps with the overall vision of the Monroe Road Neighborhood Center District; and

WHEREAS, Said Pattern Book document is prepared under the authority of the Wisconsin State Statutes, Chapter 135: Zoning of the Town of Ledgeview Municipal Code, and the Town of Ledgeview Comprehensive Plan 2035, being deemed necessary to promote the public health, safety, morals and welfare of the Town of Ledgeview; and

WHEREAS, The Monroe Road Neighborhood Center District Pattern Book may require various updates due to policy changes and/or references to Municipal Code amendments.

THEREFORE BE IT RESOLVED, That the Zoning & Planning Commission recommends approval of the Pattern Book for the Development of the Monroe Road Neighborhood Center District to the Town Board.

Zoning & Planning Commission
Town of Ledgeview

Introduced: November 16, 2016
Adopted: February 14, 2018

SIGNED:  Chair
Jane Tenor

ATTEST:  Recording Secretary
Charlotte Nelson



Mead
& Hunt