



Ledgeview Business Park Development Guidelines

July 2006

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Introduction

The Ledgeview Business Park Development Guidelines govern development and redevelopment activities on parcels within the Ledgeview Business Park. The boundaries of the business park are shown on the Future Land Use map of the Town of Ledgeview Comprehensive Plan. See the Ledgeview Business Park Master Plan for complete implementation information.

These guidelines are stricter standards for building and design, landscaping, and urban design. These guidelines have been adopted through incorporation into the implementing Planned Development District Business Park(PDD-BP).

There are three sections to these Development Guidelines.

- **Character Images:** These images depict the intended character of all areas of the business park. Future development plans should include the materials, open space design, interaction with the natural environment, and general character shown in these images.
- **Design Guidelines:** The images and text describe the required design components of development within the business park. The design guidelines speak to site design, building design, access and parking, streetscape and open space, and signage and lighting. The section begins with general design guidelines that are applicable throughout, followed by characteristic design guidelines for each district.
- **Streetscapes:** The final section describes the intended streetscapes for the existing regional roadways and new streets within business park. The guidelines include designs between the street curbs and between the curbs and the edge of the right-of-way.

Purpose

The Ledgeview Business Park Development Guidelines serve to guide land owners and prospective developers of property within the Ledgeview Business Park. These guidelines are designed to create a better quality of design, from the site to the regional level. The development guidelines aim to safeguard property values, protect public and private investments, and promote high quality commercial, residential and industrial development consistent with the desired character of Ledgeview.

Applicability

The development guidelines aim to further land use and development controls in the Ledgeview Business Park. These standards are in addition to already adopted commercial and industrial standards in the Ledgeview Code and the design standards of the Neighborhood Commercial District or Planned Development District (as applicable). All properties within the Business Park will be subject to a design review process based on criteria specified in the zoning code.

The design review process shall also be required when existing commercial and industrial uses undergo additions and alterations to roofs and exterior walls, which had previously not been subject to design review.

The development guidelines shall be adhered to before a building permit is issued to new commercial, industrial or residential development within the Business Park. Also, no site work shall be allowed such as tree removal, grading, or excavation until design approval is received.

Ledgeview Business Park



Office buildings should have variations in building massing and formal landscape with pedestrian friendly entrances.



Gas station with an interesting roof form.



Water fountains should be used as entrance landscape feature.



Natural elements such as stone and water should be used as part of formal landscape.



The use of natural materials is encouraged. Various combinations of stone, wood, water, native plants and trees shall be used to create formal and informal landscapes. The image on the left is an example of a formal landscape in a corporate campus.



Building materials such as brick, stone, exposed concrete and glass are suitable for corporate buildings. Massing is as important as the materials to create an attractive facade. Changes in building heights and setbacks help develop an attractive massing.



Natural elements should be used to create entrance signs to various districts in the business park (left).



Entrance signs using natural elements and materials help frame the entrance to a big-box retail center.



Colored pavement and landscape begins to generate a pedestrian friendly environment. Street furniture and planting beds can be used in areas such as the village center and corporate campus.



Use of natural stone to create entrance signs like this is encouraged.



The theme depicted in the Business Park stresses on natural materials, forms and textures, and colors.

From entrance signs (left) to landscape plazas (bottom left) and building facades, use of locally available materials and organic forms is encouraged.



This would help to define and reinforce the character of Ledgeview.



Big-box retail stores should develop natural landscape at entrances and also within the parking areas.



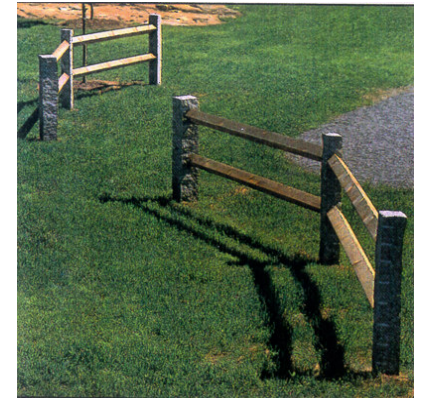
Pedestrian trails along the ravine edge help maintain a natural aesthetic. Interpretive signs describing the geology, biology or ecology of the area are helpful.



Stone and water as landscape elements in open space areas.



Formal landscape with pedestrian paths in the corporate campus space. Detention ponds become part of the landscape.



Stone fencing used to loosely define trail edges.



Pedestrian and bike trails should be encouraged in the Business Park. They shall run along the natural edge wherever possible to encourage pedestrian activity. The natural edge is the land along the streams and the ravines.



Parks and open spaces with landscape features such as water fountains and play areas for kids are encouraged. Again, the predominant materials used for landscaping are stone and native plants. Different types of open space exist in the Business Park, depending on the location and land use.



Detention ponds used to store water runoff can be integrated into the landscape either to create attractive entrances or water features.



Gateways mark the entrance to various areas. This design uses local materials and is derived from local forms.



Corporate landscape uses same elements but is more formal in character.



Variations in height and material should be encouraged. Natural materials for facades is also encouraged.



Structures shall be part of the landscape using materials such as stone and timber.



Big box retail parking areas should incorporate landscape in parking lots. This could be in the form of trees, plants or grass.



Informal pedestrian connections between different land uses can be landscaped using stone, trees and native plants.



The retail development (above) provides a good example of building massing, use of materials and landscaping. Setbacks, roof forms, materials, paving, and street trees create a high quality design.



The Village Center would consist of mainly two to three storeyed buildings with minimum setbacks, and a pedestrian friendly environment.



Water fountains and plantings help create an attractive Village Center.



Brick paving, street furniture and crosswalks help create a pedestrian friendly setting.



Textured pavement combined with rough cut stone sculptures can be used in corporate campuses.



Attractive buildings frame the main street, having a greater level of architectural and landscape detail



Roundabouts can be used as traffic calming devices and to create a focal point, either in the form of a fountain or a sculpture.



The central plaza incorporates elements such as lighting, sculptures, water fountains and planting beds. These elements combine to create an attractive congregation space in the Village Center.



General Design Guidelines

(Applicable to development activities in all districts of the Business Park)

Site Design

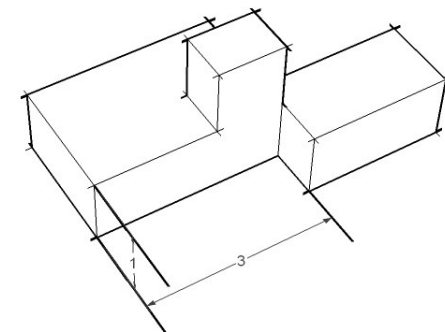
- Wherever possible, buildings should be parallel to the street.
- Locate service areas and refuse containers at the rear end of the site and screen from public view.
- Encourage storm water management treatment systems that focus on Best Management Practices (BMPs), e.g. natural landscaping to increase infiltration and reduce run-off.
- Landscape buffers and screens should be diverse in terms of elements. This shall include a combination of trees, shrubs, fences, ornamental masonry, landscape berms.
- Where appropriate, the site design should address or incorporate existing natural features such as forested areas, streams and topography.

Building Design

- All architectural elevations of buildings should consist of a base, body and cap. The base and the cap should be clearly distinguishable from the body through changes in color, material, profile or texture.
- Changes in massing should be related to entrances, the integral structure and/or the organization/architecture of interior spaces and not merely for cosmetic effect.
- Horizontal massing shall not exceed a height: width ratio of 1:3 without substantial variation in massing that includes a change in the height and building setback.
- Screen ground mounted or wall mounted equipment and architecturally integrate into building design.
- The use of low reflectance material, subtle, neutral, or earth tone colors as the predominant colors on the façade is encouraged.

Access and Parking

- Provide landscaped buffers between all public streets and building/structure or parking.
- Landscape edges of parking areas and within each parking island.
- Pedestrian access should be provided both within the site and between adjacent sites.
- Sidewalks are required on front/main entry of all buildings.
- Coordinated pedestrian circulation should also be promoted between adjacent lots.



Height:width ratio of 1:3 should not be exceeded. This can be achieved as shown above by height variations and setbacks.

General Design Guidelines

(Applicable to development activities in all districts of the Business Park)

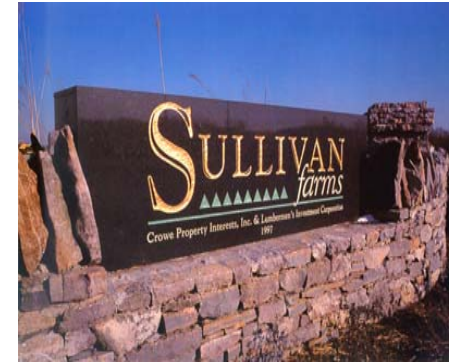
- Landscape berms, walls, fences and other forms in parking areas should be designed to allow pedestrian passage.
- Plans for parking areas should include a description of snow removal methods and storage areas such that they don't harm or kill plants.

Streetscape and Open Space

- Landscaped elements shall be replaced and maintained in a timely manner.
- All public streets should incorporate streetscape elements such as trees, street lighting, plantings, and signage.
- The property owner or lessee of the building shall be jointly responsible for maintenance of the parking area, accessways, striping, landscaping, screening and required fences.
- Convey storm water to on-site infiltration areas which should be designed as site amenities.

Signage and Lighting

- All signage should complement the theme of the business park.
- Integrate signage location, form, and material with the building design.
- Design and position signs such that they frame the front edge of the site. The same treatment should be done using street lighting such that it defines the street edge.
- Billboards and other off-site signage are prohibited.
- No sign shall be located as to restrict sight lines and orderly operation and traffic movement.
- All exterior lighting should balance on-site needs for safety, security and aesthetic effects.
- Exterior architectural lighting for buildings and landscape shall be ground-mounted. They shall be indirect, focussed in a manner that the source of light is not visible from the street or adjoining property.
- Street and sidewalk lighting should be of a complementary style and color scheme, depicting the theme of the business park.



The material, texture and color of signage should be consistent with the theme of the business park. This means more earth tone colors, rough texture and stone (mainly) signages. The examples above show different ways to achieve this.

**Highway-
Oriented
District**



Highway-Oriented District

Purpose

The design guidelines for the Highway-Oriented District aim to protect and enhance the character and quality of commercial districts where gas stations, convenience stores and outlots are located.

Site Design

- Buildings should front the public streets such as CTH MM, I-43 and USH 141 and be buffered by landscaping.
- Wherever possible, locate the gas pumps behind the store or to the side and bring the store close to the public street (CTH MM or I-43).

Building Design

- Building facades facing major roads such as CTH MM and I-43 should receive full design consideration. This means that the facades should be subdivided and proportioned using features such as windows, frames, sills and lintels, shading devices, and modulations of the walls.
- Create a design character easily utilized for a wide variety of businesses. Avoid franchise design that signifies a particular brand or product.
- Storefronts should be designed with a theme similar to the rest of the Business Park in terms of use of materials, architectural elements, building massing, and landscaping in and around the site.
- The design of pump islands should be architecturally integrated with other structures on-site using similar colors, materials and details.
- Drive through elements should be architecturally integrated into the building rather than appearing to be applied or "stuck on" to the building.



Landscape buffer between building and main street screens parking areas.



Use of materials and design elements that avoid typical franchise character

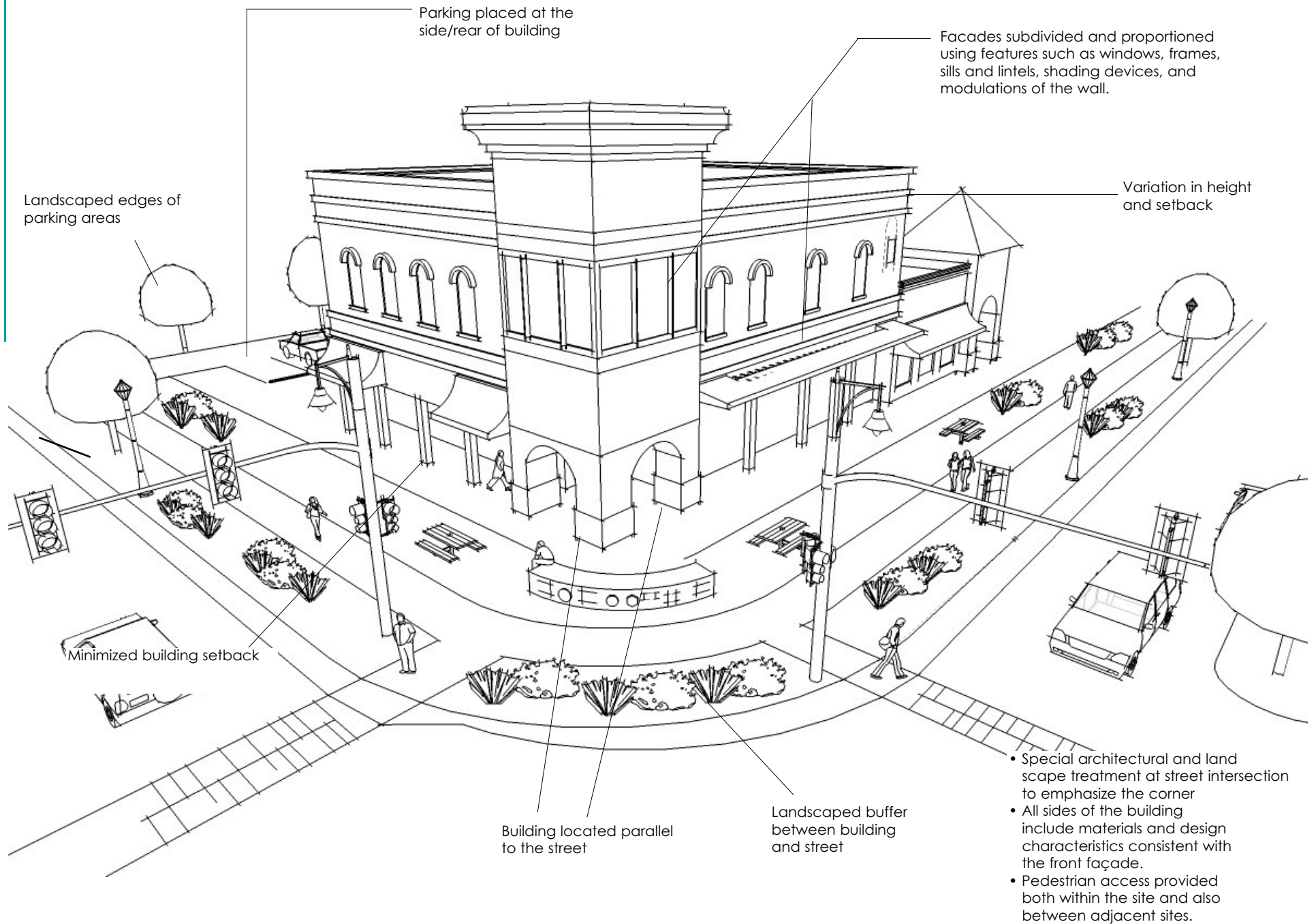


Integration of pump islands with other structures using colors, materials and details

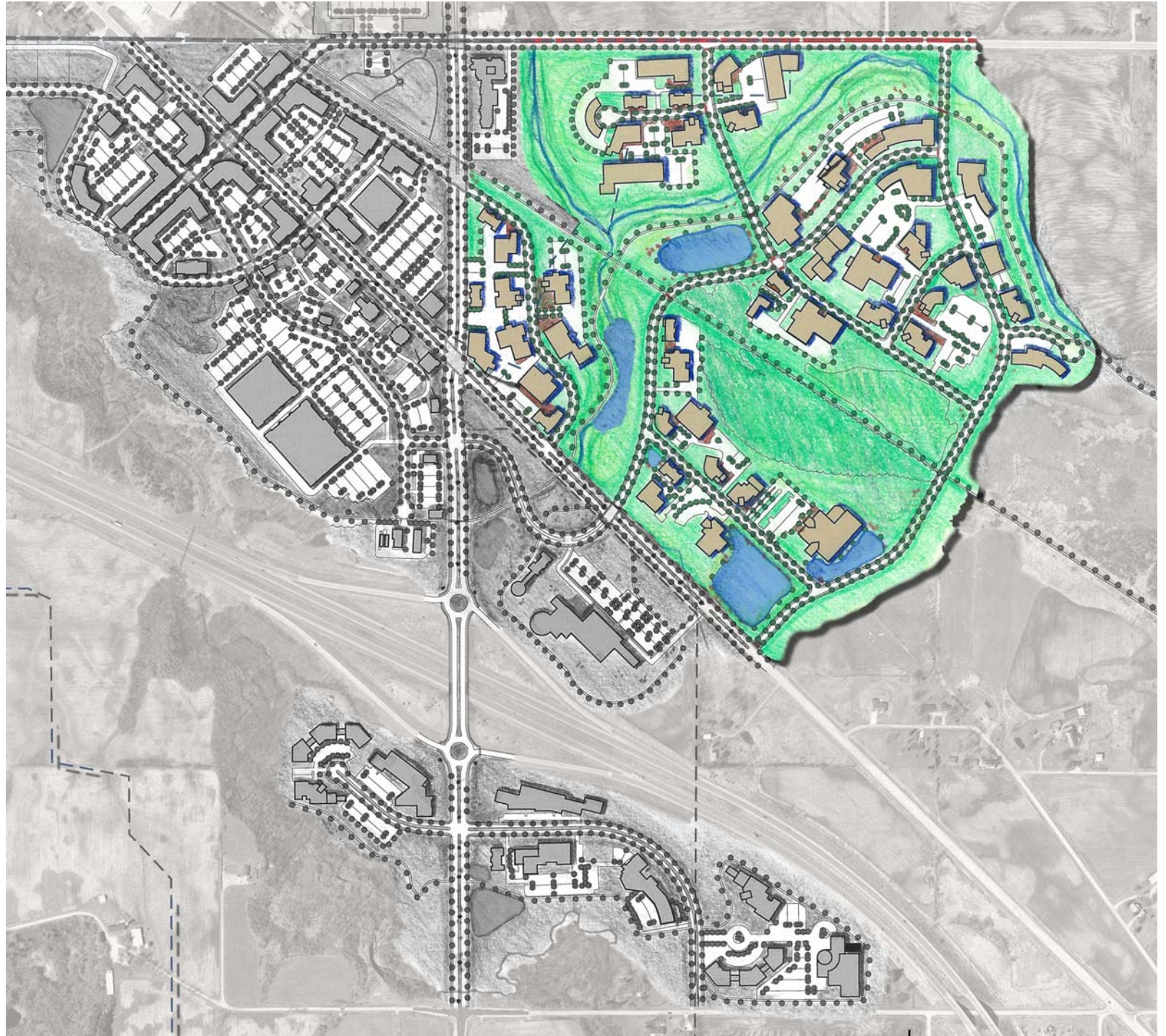
Highway-Oriented District

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**Light
Industrial
District**



Light Industrial District

Purpose

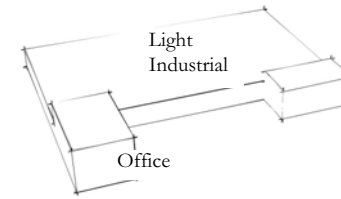
The design standards for the Light Industrial District promote a consistent and appealing site and building design while encouraging economic development.

Building Design

- Building facades facing STH 23, USH 141 and CTH MM should receive full design consideration. These sides should have detailed facades, good quality materials and construction, and incorporate landscape elements.
- Larger industrial buildings shall avoid monolithic appearance on frontages and rooflines. The larger "box" type massing of such structures must be offset by breaking up building sections, or by the use of elements such as variable planes, projections, setbacks, and changes in roof lines.
- Encourage visually interesting roofs, provide variations in the roof line and incorporate treatments such extended eaves and parapet walls with cornice treatments.
- The clustering of smaller, visually compatible structures is desired over singular large structures.
- Larger industrial buildings should have a small office component fronting CTH MM and USH 141 . This office façade shall be subdivided and proportioned using architectural features such as windows, entrance features, arcades, porches, or treillage with vines along no less than 50% of the façade.
- No single establishment shall exceed a building footprint of 150,000 square feet as defined by the exterior walls. The office component shall not count towards the 150,000 square feet.

Access and Parking

- Plant materials used for screening must be of a suitable size and density to accomplish screening within five growing seasons.
- Locate parking areas at rear of building wherever possible. Where necessary, locate parking at the side of building and provide a landscape buffer between the parking and any public street. Offstreet parking in front of buildings shall only be considered when no other design option is possible.



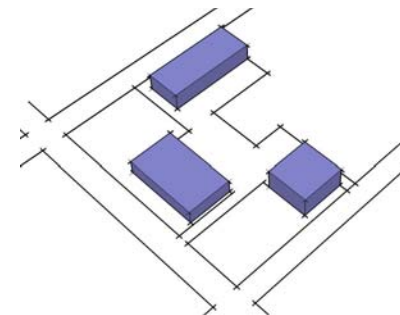
Small office component fronting public street with industrial uses behind



Office component with higher level of architectural detail and design



Screening mechanical equipment and refuse containers



Adjacent parking lots linked to promote internal circulation and shared parking

Light Industrial District

- Adjacent parking lots should be linked to provide internal traffic circulation.
- Connect building entrances to sidewalks and cross walks.
- Within the site, pedestrian access should be provided from the parking lots to building and between the different parts of a building complex.
- Off-street parking spaces shall not be used for open storage, a sale or rental of goods, or storage of inoperable vehicles without approval of the site review/zoning and Planning Commission.

Streetscape and Open Space

- Create pedestrian/bike trails through open space areas and along the wetland edge, allowing for future connections to other areas.
- Plants which provide interest in structure, texture and color should be planted along all public streets and in larger open spaces.
- Where appropriate, site design should integrate the proposed landscaping/open space with the adjacent landscaping, open space or natural feature (wetland, forested area)
- A minimum of 25% of a site should be dedicated to open space (areas excluding buildings, roads and parking areas). 40% is considered desirable.
- Landscape private streets with street trees.
- In cases where large buildings are set back, add dense landscaping such that they define the street edge.

Signage and Lighting

- Signage should indicate only the name of the business, nature of business and/or the street address.
- All exterior lighting should be a part of the architectural and landscape design concept in color, location and type of lighting.
- Promote lighting fixtures which complement the street lights in terms of materials, form, color.



Landscaped parking edge



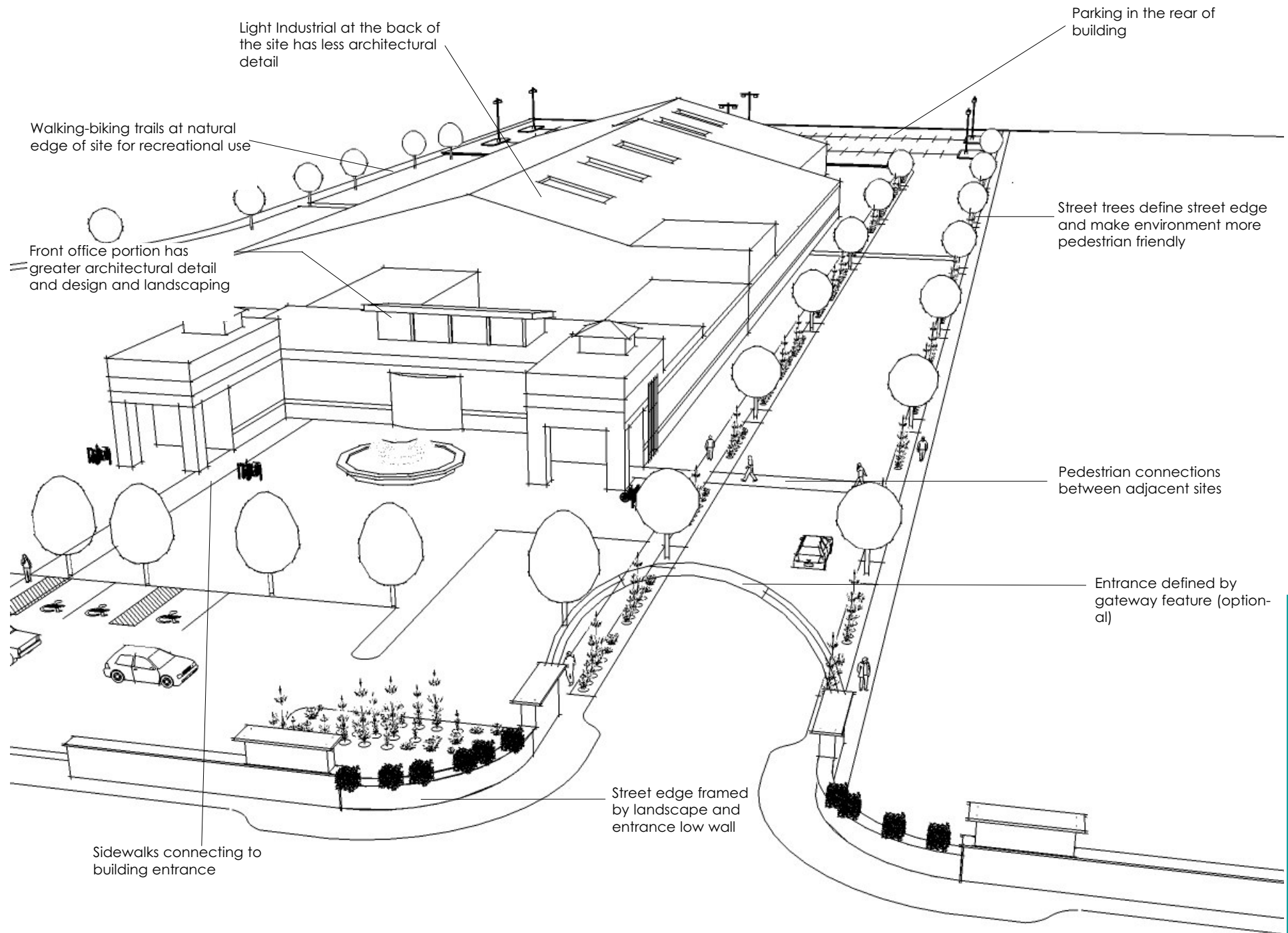
Pedestrian trails along ravine edge



Incorporating storm water ponds into landscape



Use of color, material and landscape should be similar to the buildings in the District



**Corporate/
Support
District**



Corporate/ Support District

Purpose

This district is part of the entrance corridor into the Ledgeview Business Park and Green Bay region. Large corporate campuses with high quality design along I-43 should aim to create an attractive and desirable gateway.

Building Design

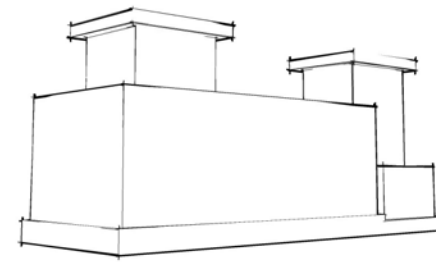
- High quality architecture should be promoted as the buildings are within a gateway viewshed into the Town of Ledgeview. Quality in terms of style, construction, materials and landscape.
- The use of high quality building materials such as brick and native stone is encouraged.
- Buildings occupying more than 25,000 square feet and/or 60 feet or more of street frontage should be designed with recess and projections, material changes, and other articulations every 30 to 60 feet. This helps break up large masses and creates the appearance of smaller buildings.
- In order to promote human scale, large blank facades need elements that provide visual interest. Human scale and visual interest can be achieved if the façade is subdivided and proportioned using features such as windows, frames, sills and lintels, shading devices, and modulations of the wall.
- Encourage visually interesting roofs, provide variations in the roof line and incorporate treatments such as extended eaves and parapet walls with cornice treatments.

Access and Parking

- Provide a landscaped buffer between I-43 and CTH MM and building/structure or parking.
- Locate parking areas at rear of building wherever possible. Where necessary, locate parking at the side of buildings and provide a buffer between the parking and public street.
- Adjacent parking lots should be linked to provide internal traffic circulation.



High quality architecture in terms of design, materials and construction quality.



Variations in roofline and building massing helps create a visual interest.



Above and Below: reducing run-off in parking lots using landscaped islands



Town of Ledgeview

Corporate/ Support District

Streetscape and Open Space

- The site should incorporate elements such as trees, lighting, plantings, and signs.
- Trees should be planted at a maximum distance of fifty feet from one another along the interior roads (same as PDD).
- Pedestrian trails should be created along the ravine edge for private recreational purposes.
- A minimum of 40% of a site should be dedicated to open space (areas excluding buildings, roads and parking areas).

Signage and Lighting

- All ground signs should be monument signs.
- All exterior lighting must be a part of the architectural and landscape design concept in color, location and type of lighting.

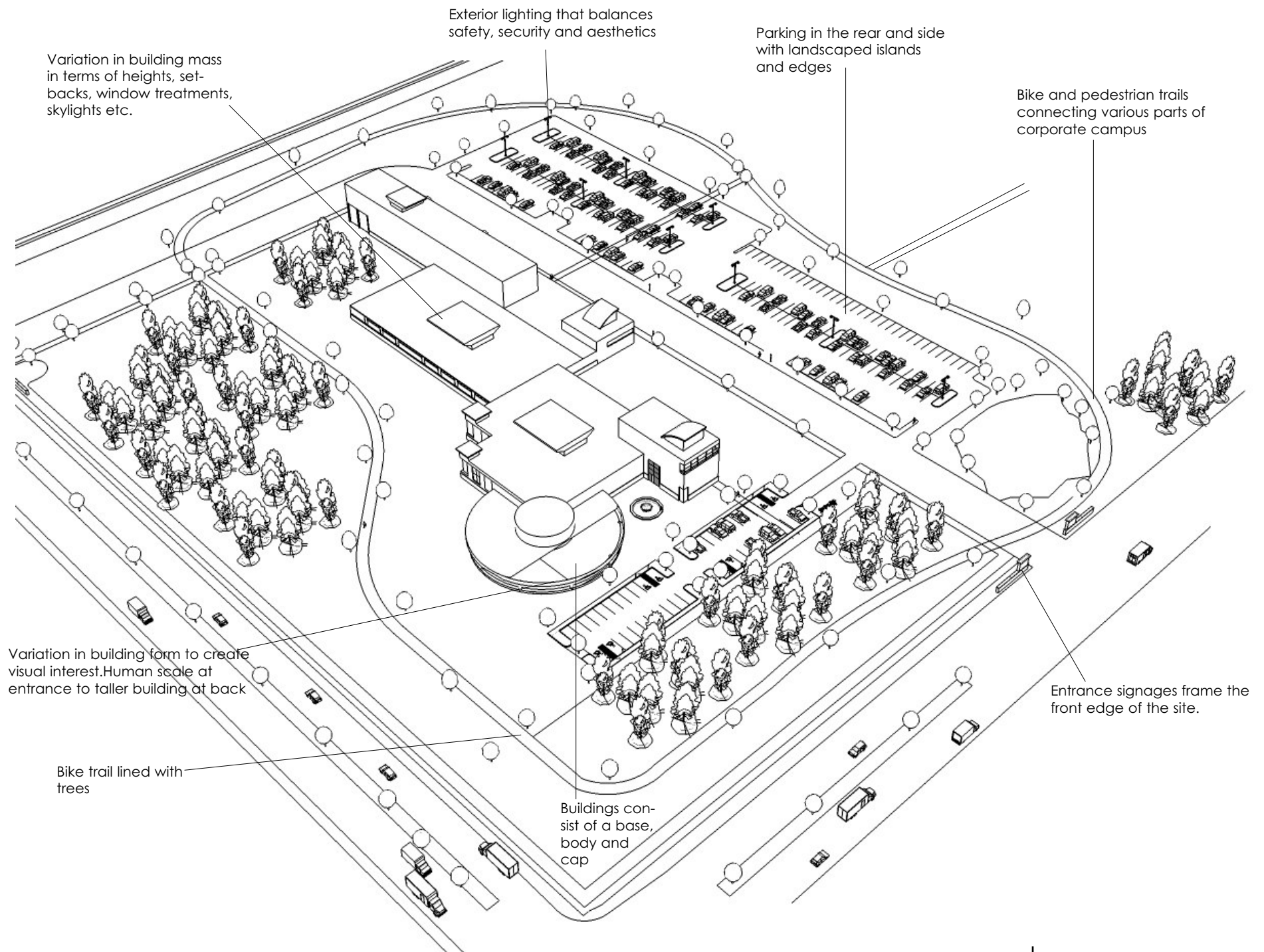


Landscape integrated with detention ponds on site



Above and Below: Entrance signs based on themes





***Pedestrian
Scale
Mixed-Use
District***



Purpose

The principal purpose of this district is to provide for a variety of pedestrian-oriented retail, lodging, residential and civic land uses in the town center area.
(from Ledgeview Code)



Pedestrian Scale Mixed-Use District

Please refer to the 'Code of the Town of Ledgeview' 135-189 - 135-196 for adopted standards and the Town of Ledgeview 'Neighborhood Center District Concepts for Development' (Feb. 21,2006) within the Master Plan Appendix.



Town of Ledgeview

**Big-Box
District**



Big-Box District

Purpose

The Big Box District allows large scale retail formats, but requires a higher design quality and sensibility consistent with the Business Park.

Site Design

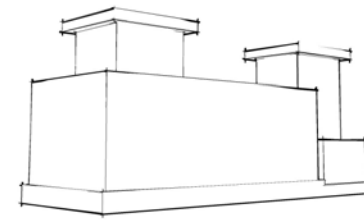
- Encourage storm water management treatment systems that focus on Best Management Practices (BMPs), e.g. natural landscaping of parking areas to reduce run-off.
- Detention basins should be incorporated into the site design in the form a landscape element.

Building Design

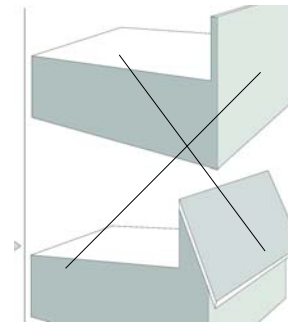
- In order to promote human scale, large blank facades need elements that provide visual interest.
- Human scale and visual interest can be achieved if the façade is subdivided and proportioned using features such as windows, frames, sills and lintels, shading devices, and modulations of the wall.
- The clustering of smaller, visually compatible commercial structures is desired over singular large structures fronting the public street.
- Each large retail store shall have clearly defined, highly visible customer entrances featuring architectural elements such as canopies or porticos, overhangs, arcades, raised parapets, arches or roof forms.
- Discourage fake parapets and facades and create architectural elements which serve a utilitarian purpose, not merely cosmetic.
- Screen ground-mounted or wall-mounted equipment.

Access and Parking

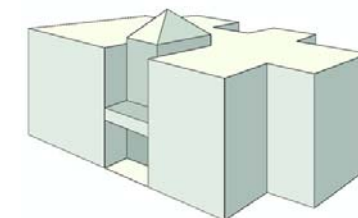
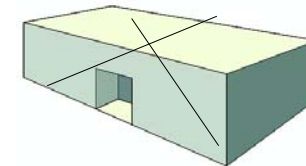
- Provide a landscaped buffer between all public streets and building/structure or parking.
- Allow parking in the front of the store, but break up the parking lot with landscaped islands, trees and plantings.
- Landscape edges of parking areas at in-between islands.



Variations in building height helps to create an attractive building form.



Do not use fake parapets and facades



Avoid large dominant masses by creating variations in setbacks, roof forms and heights.

Big-Box District

- Pedestrian access should be provided both within the site and also between different sites. Within the site, pedestrian access should be provided from the parking lot to the store. Coordinated pedestrian circulation should also be promoted between adjacent lots.
- Adjacent parking lots should be linked to provide internal traffic circulation.
- Off-street parking spaces shall not be used for open storage, sale or rental of goods, or storage of inoperable vehicles without approval of the site review/zoning and Planning Commission.

Streetscape and Open Space

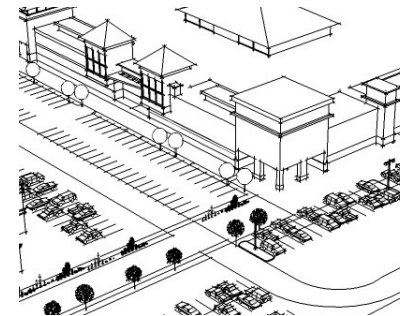
- The site should incorporate elements such as trees, lighting, plantings, and signs.
- Trees must be planted at an approximate minimum distance of fifty feet from one another along the interior roads.

Signage and Lighting

- All exterior lighting must be a part of the architectural and landscape design concept in color, location and type of lighting.
- Lighting should be designed in a manner which does not permit an adverse effect upon neighborhood properties, especially residential property.
- All ground signs should be monument signs.
- Lighting on site should be used judiciously as needed for safety and information.



Parking lots interspersed with landscaped islands create a better visual character, pedestrian friendly environment and reduce storm water run-off.



Landscape parking areas to break up large lots



Incorporate elements such as signs, public sidewalks, pedestrian access and landscaped buffers between road and building

Variation in massing, window sizes and placement, materials, rooflines help create a better facade

Setbacks in building massing

Architectural design using setbacks, elements such as windows, skylights, roof forms

Parking concealed in the rear

Landscaping to buffer buildings from main street and also to create a pedestrian-friendly environment

Public Road

Pedestrian connections within and to site

Entrance signages frame the front edge of the site.

Outlots front main street and partially conceal parking and big box behind

