

TOWN OF LEDGEVIEW

# LEDGEVIEW BUSINESS PARK MASTER PLAN

## DEVELOPMENT STRATEGY AND ACTION PLAN



ADOPTED

INSERT ADOPTED DATE, 2006



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Ledgeview Business Park Development Guidelines  
Town of Ledgeview Neighborhood Center District Concepts for Development (February 21, 2006)  
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Town of Ledgeview Resolution

## APPRECIATION

Schreiber/Anderson Associates wishes to thank the Town of Ledgeview for its assistance in preparing this master plan. The input and review by interested citizens, stakeholders, and the Planning and Town Boards. In particular, SAA wishes to thank Sarah Burdette, Clerk/Administrator, and Scott Brosteau, Mead and Hunt.

Planning Assistance Provided by:



Schreiber/Anderson Associates  
717 John Nolen Drive  
Madison, WI 53713





## Executive Summary

The Town of Ledgeview intends to develop a business park near the CTH MM interchange with Interstate 43, the Town's only interstate interchange. The business park will be a well-designed mixture of light industrial uses, corporate/support, a large-format retail site with outlots, and a pedestrian-scaled mixed use area. Preserved and enhanced natural areas – ravines, streams, a regional bike path, meandering trails, and a trailhead park – help to define a natural character for the business park.

This master plan outlines the steps necessary to support and complete the development of a business park. This master plan has three sections and three appendices. An introductory section describes the existing conditions of the area and planned and expected infrastructure improvements. The second section describes the intended character and land uses. The final section of the master plan is a detailed, phased implementation schedule, including estimated costs of improvements, funding opportunities, a phasing plan, and a marketing strategy.

The appendix contains three documents. The first section is the Ledgeview Business Park Development Guidelines, which describes the expected character and level of development, depicted in many images and drawings. The development guidelines also include streetscaping designs for all internal and adjacent streets. The Neighborhood District Design Concepts, the second section of the appendix, serves the same role for the pedestrian-scaled mixed-use areas of the business park. The third section of the appendix provides detailed cost estimates for public and private infrastructure to enable and support business park development.

## Introduction

The Town of Ledgeview hired Schreiber/Anderson Associates (SAA) to plan a business park near Interstate 43 and CTH MM, the Town's only interstate interchange. The area is currently a combination of agricultural and unbuildable open space, with some existing highway-oriented and neighborhood businesses and residences.

The vision for the Ledgeview Business Park identifies it as a signature entrance into the Town of Ledgeview and the Green Bay region, consisting of a variety of commercial uses including corporate, light industrial, highway commercial, and pedestrian-scale neighborhood retail.

### *Intents*

The Town of Ledgeview's intent is to adopt a master plan for a functional, efficient, and attractive business park before water and sewer services are extended into the area. The Town seeks to capitalize on the business park's location at an interchange at the southeastern edge of the Green Bay region to support economic development. An intent of the business park is to encourage growth in generated property and sales taxes. The Town's vision for the business park as one that contains uses that "provide natural landscaping, quality onsite signage, minimal parking, and adequate stormwater management."

### *Process*

SAA led an interactive process to conceive, plan and design the business park. The process included the following broad steps:

- Base mapping, background data collection, and an analysis of existing conditions
- Creation of two concept plans and pictorial descriptions of desired design character
- Creation of a preliminary master plan, schematic civic and streetscape designs, aerial perspective and development guidelines

SAA coordinated closely with the Town, stakeholders and Town residents (as follows):

- A technical resource team comprising of representatives of the Town and the Town's engineer guided the process throughout.
- Stakeholder interviews (SAA met individually with 15 stakeholders in December 2005)
- Informal survey of local commercial real estate professionals to assess market conditions
- Public meetings on December 12, February 9, and April 20



*SAA conducted three public meetings at Ledgeview Town Hall.*

In response to the ideas and direction SAA received from the Town, interviewed stakeholders, Town residents, and the existing conditions analysis, SAA prepared multiple concepts plans, with variations on the future business park's land uses, access, entryways and gateways, streetscape and amenities, street layout, bicycle and pedestrian linkages, landscaping and theme and identity ideas. To maximize the Town's goal that the business park should be dedicated to land uses that generate tax surpluses for the Town, the master plan minimizes residential uses except where only residential uses are appropriate.

## ***Existing Conditions Assessment***

There is a short-term need for a business park master plan. The business park area is primarily undeveloped agricultural fields with some highway-oriented commercial and some homes. However, the location at an interstate interchange and the imminent extension of water and sewer suggest that development will be attracted to this area and that the existing character will soon change.

This existing conditions analysis examines an area larger than the designated Future Business Park. It analyzes the area approximately bordered by Hidden Valley Lane to the west, Driftwood Lane to the south, the intersection of Main Street and Maple Court to the east, and Meadow Circle in the Village of Bellevue to the north. Figure 1: Existing Conditions, summarizes the existing conditions assessment.

### ***Transportation***

The study area has multiple state and county roadways but limited local roadways. There are three state routes. Interstate 43 bisects the area, and only three roads provide mobility across the interstate: CTH MM (at an interchange), Glenmore Road, and Shadow Lane (the latter two pass under the interstate). US 141 begins at the Interstate 43/CTH MM interchange, proceeds northwest on Main Street, and joins the STH 29 alignment into the Green Bay area. STH 29 follows Kewaunee Road from Kewaunee (forming the Bellevue/Ledgeview border until Manitowoc Road), and then turns southwest. The STH 29 designation turns right and follows US 141 to the northwest.

The area has two county routes. CTH MM, also called Dutchman Road, is the north-south corridor through the study area. CTH MM ends at CTH R to the north and connects to CTH G to the southwest. CTH R, also known as Main Street, connects Denmark and the Green Bay area. It shares roadway with US 141 at the CTH MM termination, and with STH 29 at the termination of Kewaunee Road.

Other than the state and county routes, there are limited local roadways. Most are short dead-end streets, but three provide connections. Glenmore Road is a major north-south route, the only continuous north-south route between East Mason Street (at the I-43 Business Park) and Shirley Road (STH 96). Shadow Lane connects CTH MM and CTH R. Wall Street is a very short connection between CTH R and Kewaunee Road.

Brown County long-range transportation plans include the completion of the Eastern Arterial (CTH EA) between STH 54/57 and STH 29 by 2007. The Brown County Highway Department and the Village of Bellevue are negotiating the cross-section for the segment; Brown County and WisDOT prefer a four-lane divided highway while the Village prefers a narrower cross-section. It is anticipated that CTH EA will be extended to the I-43 interchange, perhaps with a realignment of STH 29.

The Fox Valley and Western freight rail line crosses the study area. It currently provides service for one user in the Denmark area, but has very low usage. There are no active railroad sidings in the study area. The Town expects that once the rail line is no longer in use, Brown County and/or the Wisconsin Department of Natural Resources will purchase the rail line right-of-way and convert it to a multi-use recreational trail.

There are no bicycle facilities in the study area. The Brown County Bicycle and Pedestrian Plan Update (June 1998) designates CTH MM to include paved shoulders to connect planned bicycle facilities on CTH G and the Eastern Arterial (CTH EA). With the exception of a disconnected portion along STH 29 west of Cottage Road, there are no sidewalks in the study area.

The study area is not in the Green Bay Metro transit service area. The closest fixed route transit service is Route 14, which serves the I-43 Business Park. Since Ledgeview is not in the transit service area, there are no paratransit services available. In the long-term, it is possible that transit service could be extended to the I-

43/CTH MM interchange via Interstate 43 or the Eastern Arterial if there is a concentration of employees and those employers are willing to subsidize service.

There are no official park-and-ride lots in the study area. The edge of Cedar Hedge Lane serves as an informal park and ride lot. Both the Town of Ledgeview and the Wisconsin Department of Transportation have expressed an interest in establishing a park and ride in the interchange area.

### *Open Space*

The only designated public open space in the area is Fonferek's Glen. The 74-acre county park is located on CTH MM in the extreme southwest corner of the study area. It provides parking and an overlook of a scenic 30-foot waterfall.

In its comprehensive plan, the Town of Ledgeview proposes trails along Bower Creek west of the Glenmore Road. The ravines east of Glenmore Road and south of CTH R also provide an opportunity for hiking and walking trails.

### *Significant Viewshed*

The I-43/CTH MM interchange is a gateway for the Town and the entire Green Bay region for travelers coming from Manitowoc, Sheboygan, Milwaukee, and Chicago. Due to the terrain and the curvature of I-43, the northeast and southeast quadrants of the I-43/CTH MM interchange are highly visible and thus particularly important for the Town. Figure 1: Existing Conditions depicts the gateway viewshed.

### *Stormwater*

The area's current stormwater system is comprised of a conveyance system of swales and roadside ditches. The area's ravines and low-lying wetlands also serve as part of the stormwater system. The Town, through its engineer Mead and Hunt, has completed a Town-wide stormwater management plan. Mead and Hunt has preliminarily located potential stormwater pond sizes and locations in the study area, and they are depicted on the Figure 1: Existing Conditions. The stormwater management plan did not include a complete environmental assessment of the area so the exact locations of all wetland and similar areas are not known. As development occurs, further delineation will take place and minor modifications to the preliminary stormwater ponds is expected.

### *Utilities*

Regional and local utility authorities are extending both sanitary sewer and municipal water into the area. The intent of this master planning process is to prepare for expected development once these utilities are established.

Sanitary sewer service is currently provided to parcels along Wall Street and STH 29 north of CTH R. Service is provided by the Village of Bellevue, through an agreement with the Ledgeview Sanitary District and the Green Bay Metropolitan Sewerage District. Through the Northerly Interceptor project, the Town is extending sanitary sewer service to a majority of the designated business park within the next year. Upon completion of the project, all four quadrants of the I-43/CTH MM interchange will have sewer service. Figure 1: Existing Conditions depicts the Northerly Interceptor route and sewer service area.

In 1999, six Central Brown County communities – City of De Pere, Villages of Allouez, Bellevue, and Howard and the Towns of Lawrence and Ledgeview – formed the Central Brown County Water Authority (CBCWA) to develop a solution for meeting long-term drinking water needs. These communities share a common groundwater source that cannot meet their future needs. The CBCWA signed an agreement with

Manitowoc and Manitowoc Public Utilities in July 2004 to obtain fresh water from Lake Michigan. Construction of the project including the water distribution system, water transmission main, and increased microfiltration plant capacity started in July 2005. Construction is expected to take 12 to 14 months, with Lake Michigan water flowing to the communities by December 2006. Pipeline construction in the Town of Ledgeview and Village of Bellevue is part of Contract A, which is currently underway, including a metering station on CTH MM 600' south of Shadow Lane. A water tower and fire station are planned for the same location. Figure 1: Existing Conditions depicts the CBCWA pipeline route.

### *Land Use*

The entire designated business park area is within the 0-5 year growth increment as designated in the Town's comprehensive plan. Within the Town of Ledgeview portion of the study area, there is a mixture of land uses:

- *Agriculture*: The majority of the study area is in agricultural production. Farms that are not designated Future Business Park and that are north of Interstate 43 retain agricultural designation on the Town's Future Land Use map. Most agricultural land north of Driftwood Lane and south of Interstate 43 has been designated Future Neighborhood Residential on the Future Land Use map.
- *Large-lot residential lots* are scattered throughout the area, with concentrations on Hidden Valley Lane, Glenmore Road, and Shadow Court. Private well and septic systems serve these larger homes.
- *Smaller-lot residential lots* are located along Wall Street, Buddy Lane, and Cottage Road. Public sewer serves these homes.
- *Highway-oriented commercial uses* are located west of and directly adjacent to the Interstate 43 ramps. The northwest quadrant features a gas station, convenience store, and fast food restaurant. The southwest quadrant features a truck maintenance and repair shop, a closed restaurant, and a car sales lot.
- *Neighborhood commercial uses* are within the triangle formed by CTH R, Wall Street, and the rail line. The business types vary, including a restaurant/bar, a supper club, a nursery, a bank branch, and a real estate office. These businesses primarily serve the nearby neighborhoods in the Village of Bellevue. Public sewer serves these commercial uses.
- *Warehouses* are also located within the triangle formed by CTH R, Wall Street, and the rail line. The buildings currently warehouse paper products.

There are developed lands north of the area in the Village of Bellevue.

- *Residential lots* are located along Manitowoc Road and on Meadow Circle.
- *Small-scale commercial buildings* are located on STH 29 west of Cottage Road. A vacant parcel in the northwest corner of STH 29 and Cottage Road is designated intense commercial on the Village's draft Future Land Use map.
- *Agriculture* is the primary use in the northeast corner of STH 29 and Cottage Road, with the exception of the corner where there is a VFW hall. The agricultural land is designated commercial park on the Village's draft Future Land Use map.

There is a variety of zoning districts currently regulating development in the business park area. Current zoning in the designated business park does not adequately support commercial or mixed-use development. Zoning districts currently in place include Residential (R-1), Rural Residential (RR), Exclusive Agriculture (A-1), Planned Development District (PDD), General Business (B-2), Intensive Business (B-3), and Light Industrial (LI).

### *Potential Conflicts*

The Town of Ledgeview comprehensive plan designates a business park in the general area between the I-43/CTH MM interchange and the Village of Bellevue border. The Future Business Park designation leaves out all non-agricultural parcels, resulting in a patchwork of Future Business Park areas that are not always connected. For example, a small piece of unconnected Future Business Park area at the northeast quadrant of I-43 overpass at Glenmore Road is directly opposite a future neighborhood, resulting in a potential conflict.

North of CTH R, there is a potential conflict between the business park and existing uses, in particular, the existing residential uses. South of Interstate 43, the ravines are effective buffers and transition areas between the business park and existing and future residential areas. Future Business Park designated land is adjacent to environmental areas throughout the area, with some fingers of developable land surrounded by ravines or wetlands.

The Village of Bellevue's proposed commercial park in the northeast corner of STH 29 and Cottage Road can function as an extension of the Town of Ledgeview's future business park, if there are adequate connections and a strong light industrial market.



# EXISTING CONDITIONS

## LEDGEVIEW BUSINESS PARK

Town of Ledgeview  
Brown County, WI

Project # 2134  
February 20, 2008



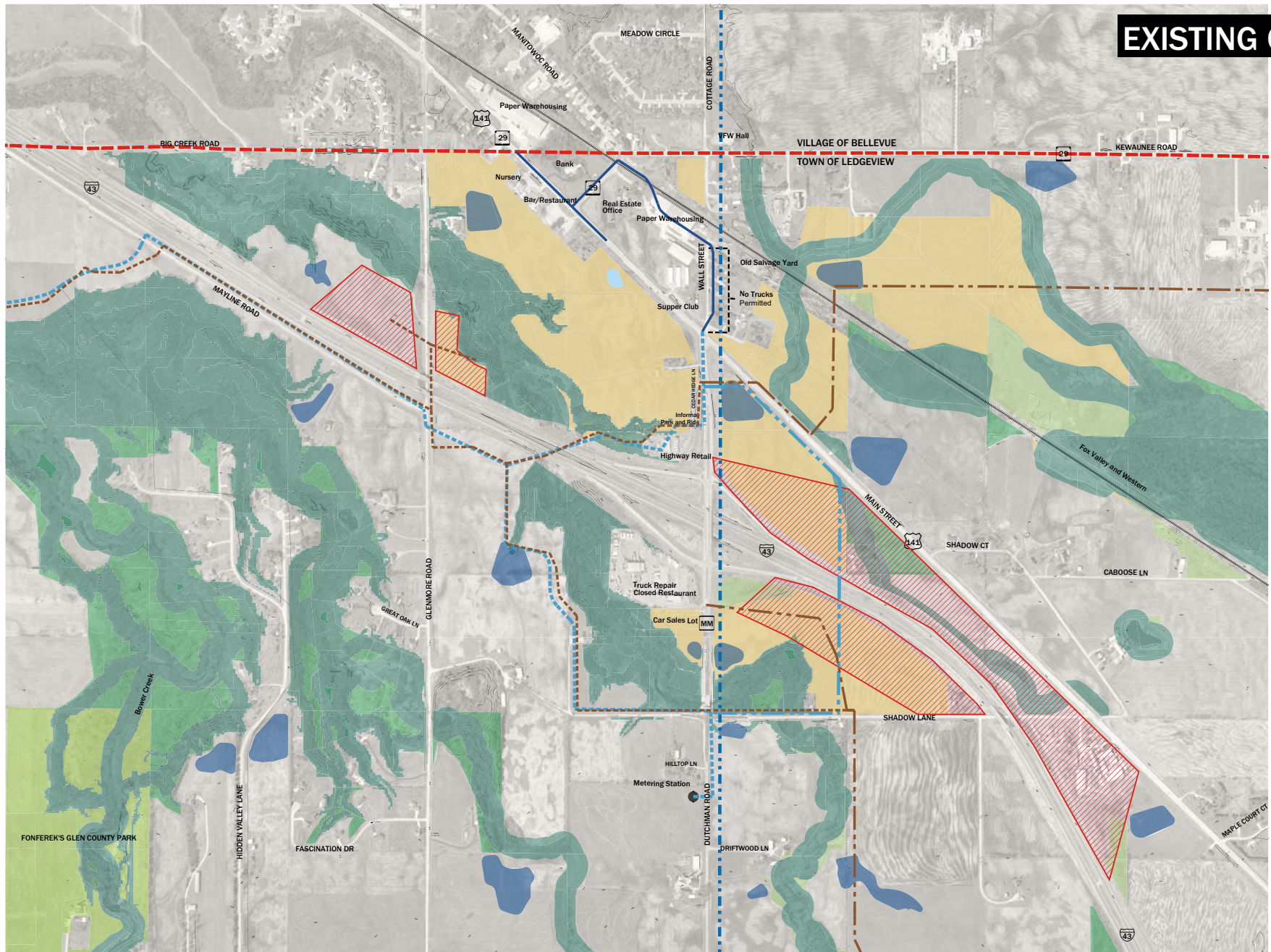
### Legend

- Municipal Boundary
- Rivers and Streams
- Existing Natural Areas / Woodlands / Environmental Areas
- Existing Recreational Areas
- Future Business Park
- Potential Stormwater Pond Locations (Draft Stormwater Management Plan)
- Gateway Viewshed
- Existing Water Main
- Proposed Water Main (Central Brown County Water Authority)
- Proposed Water Main
- Future Water Main
- Proposed Sanitary Sewer
- Future Sanitary Sewer



0 400 800  
SCALE 1"=400' 0"  
(SCALE INDICATED FOR 24"x36" SHEET)

Figure 1







## Master Plan Summary

The master plan considers the entire I-43/CTH MM interchange area, including parcels that are designated Future Business Park on the Future Land Use map of the Comprehensive Plan as well as adjacent parcels that are currently not designated Future Business Park. The Implementation section of this master plan encourages changes to the Future Business Park designation to create a contiguous and logical business park boundary. Figure 2: Master Plan includes all areas within the new recommended boundaries.

### *Planned and Assumed Improvements*

The master plan includes all existing conditions and the following planned or assumed improvements:

- The Eastern Arterial connection is complete, linking Bellevue to the CTH MM interchange along the current Cottage Road and Wall Street corridor. STH 29 is redirected to follow the Eastern Arterial to join Main Street.
- The rail line is no longer in use and has become a multi-use trail.
- Full utilities are provided to all business park sites.
- The preliminary stormwater pond size and general locations from the stormwater management plan are set, but the specific location and design are flexible.
- The Village of Bellevue has developed a commercial park at the northeast corner of STH 29 and the Eastern Arterial, as shown on the Village's draft Future Land Use map.

### *Key Master Plan Recommendations*

Figures 3 and 4 show the business park character areas and the relative acreage for each. In general, the master plan shows a significant area for corporate/support and light industrial uses, with designated locations for retail areas, both auto- and pedestrian-oriented. Figure 4: Land Use Allocation shows the approximate land use acreages for the recommended business park boundaries depicted on the master plan, as a percentage of 370 acres.

- Given the Town's desire for business uses at the interchange and the relatively stronger market for light industrial uses, the majority of the area is shown as corporate/support and light industrial uses.
- Corporate/support parks occupy significant I-43 viewsheds.
- Nearly the entire area east of the Eastern Arterial, north of Main Street, and south of STH 29 is shown as a large light industrial park, split by the multi-use trail and environmental corridors. Given frequent street connections across STH 29, this light industrial area will blend and complement the commercial park planned in the Village of Bellevue.
- Interstate-oriented retail (e.g. fast food and/or convenience store with gas) will likely expand directly adjacent to the interchange.
- There is an approximately 22-acre area reserved for a large-format retailer (big box). The area backs up to ravines and is fronted by out-lot type commercial.
- Out-lot type commercial uses are at the southwest corner of Main Street and the future Eastern Arterial.
- At the intersections of STH 29 with Manitowoc Road and Main Street, pedestrian-oriented development is adjacent to existing Bellevue neighborhoods.
- There are appropriate buffers between the business park uses and existing and future large-lot residential uses. In most places, the dividing line between commercial and residential occurs at the edge of the business park and the transition must occur on residential areas outside the park.
- Roundabouts are placed on CTH MM intersections at both interchange ramps. A formal park and ride is located near the current informal park and ride area in the big box parking lot.
- A trailhead park is located on Cottage Road along the rails-to-trails conversion.
- Bicycle trails are shown along the ravines and edges of environmental corridors north of I-43 and on the Eastern Arterial.

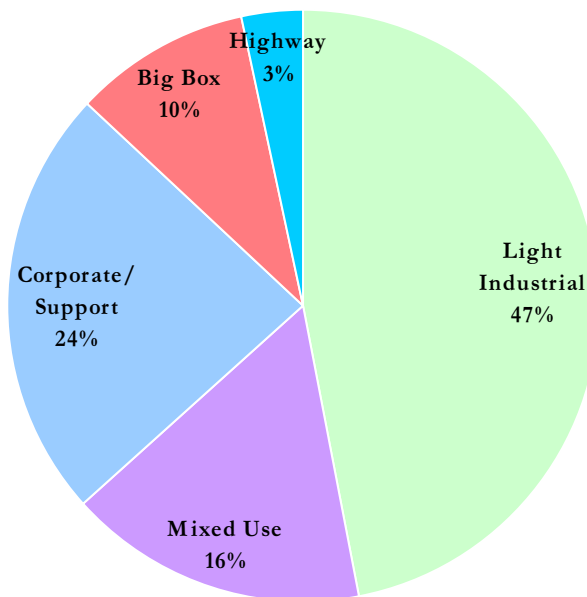
### ***Desired Development Character***

There is a desire by the Town that the appearance and the design within the business park should be higher than the remainder of the Town. To meet that goal, the Town has established guidelines that set minimum standards for open space, landscaping, architectural standards, signage, parking lot size and design, transportation connections, and buffers/fences/walls.

Development in the business park must meet the design requirements found in the Ledgeview Business Park Development Guidelines, as adopted. The images in the development guidelines and in this master plan reflect a range of development options for the business park, based on sound architectural and urban design principles. The Town should require these higher design standards but also balance the need to enable efficient traffic circulation for both personal vehicles and trucks and to allow a wide variety of light industrial uses where appropriate. Below are the key design principles or design elements discussed for each character area:

- *Light Industrial/ Small Office Light Industrial:* The front facades of light industrial buildings should be designed such that they create an attractive frontage to the two major corridors, CTH MM and I-43. This can be done with a combination of good site and landscape design, such as placing the office component of the industrial use at the front end and the industrial component at the back of the site with employee parking.
- *Corporate/ Support:* Corporate/support use occupies a majority of the gateway viewshed. Therefore, it is important to design buildings with a visual appeal, with primary emphasis on massing, landscaped setbacks, and use of high quality facade materials.
- *Pedestrian-Scale Mixed Use:* The neighborhood center should feature pedestrian-scaled development and activities. The theme for this area should be set by higher density development consisting of two- to three-story buildings lining the primary pedestrian streets leading to a community open space.

**Figure 4: Land Use Allocation**



The architectural style, scale, and landscape elements should support an environment with pedestrian-friendly retail on the first floor and residential or office uses above.

- *Big Box Commercial:* Big box commercial buildings typically lack façade design and comfortable pedestrian access. The business park development guidelines demonstrate ways to design the facades of big boxes to make them more visually appealing, ways to incorporate landscape elements in parking lots, and methods to designate clear pedestrian pathways through parking lots to main building entrances.
- *Outlot Commercial and Highway Commercial:* The development guidelines require outlot commercial sites have shallower front setbacks, buildings be oriented to the street, clear entrances, pedestrian access to major streets, and parking located to the side or rear of the building. Landscaping around the building and in the parking area creates a better-looking site and mitigates some adverse environmental effects caused by development. For highway commercial uses such as gas stations and convenience stores, the same site layout principles are applicable. In addition, adding architectural interest either in the design or materials used for canopies and the store enhances the visual quality of the overall site.
- *Open Space and Parks:* The business park should have three different typologies of open space. The first is the formal landscape, primarily in corporate/support sites along the gateway viewshed on I-43. The nature of this landscape is primarily visual with some recreational value in the form of trails and gathering spaces. The second is the public open space of the trailhead park, shown on the master plan as the triangular site at the intersection of the Eastern Arterial and STH 29. The semi-formal public open space could have a combination of natural and man-made landscape elements. For example, the public open space could serve as a community space for outdoor events in the summer.

The third type of open space is the existing natural landscape. Parks and recreation facilities such as trails and playgrounds along the ravine could help preserve and utilize the natural edge. The parks and open spaces will enhance the image of the business park.

- *Streetscape:* The development guidelines improve the aesthetics and pedestrian orientation of main streets. Design elements such as landscaped medians, boulevards, roundabouts, and sidewalks enhance the pedestrian's experience and add visual quality to the streets. In addition, elements such as streetlights, textured pavements, and street furniture improve aesthetics.
- *Gateways:* There are multiple possible locations for gateway features. Each gateway can mark the entrance into the entire Green Bay region, the Town of Ledgeview, the business park, or a particular area within the business park. No matter its location and purpose, the business park's gateways should depict the theme of the business park. The materials, scale, location, and integration with existing landscape elements generate visual appeal.



# MASTER PLAN

## LEDGEVIEW BUSINESS PARK

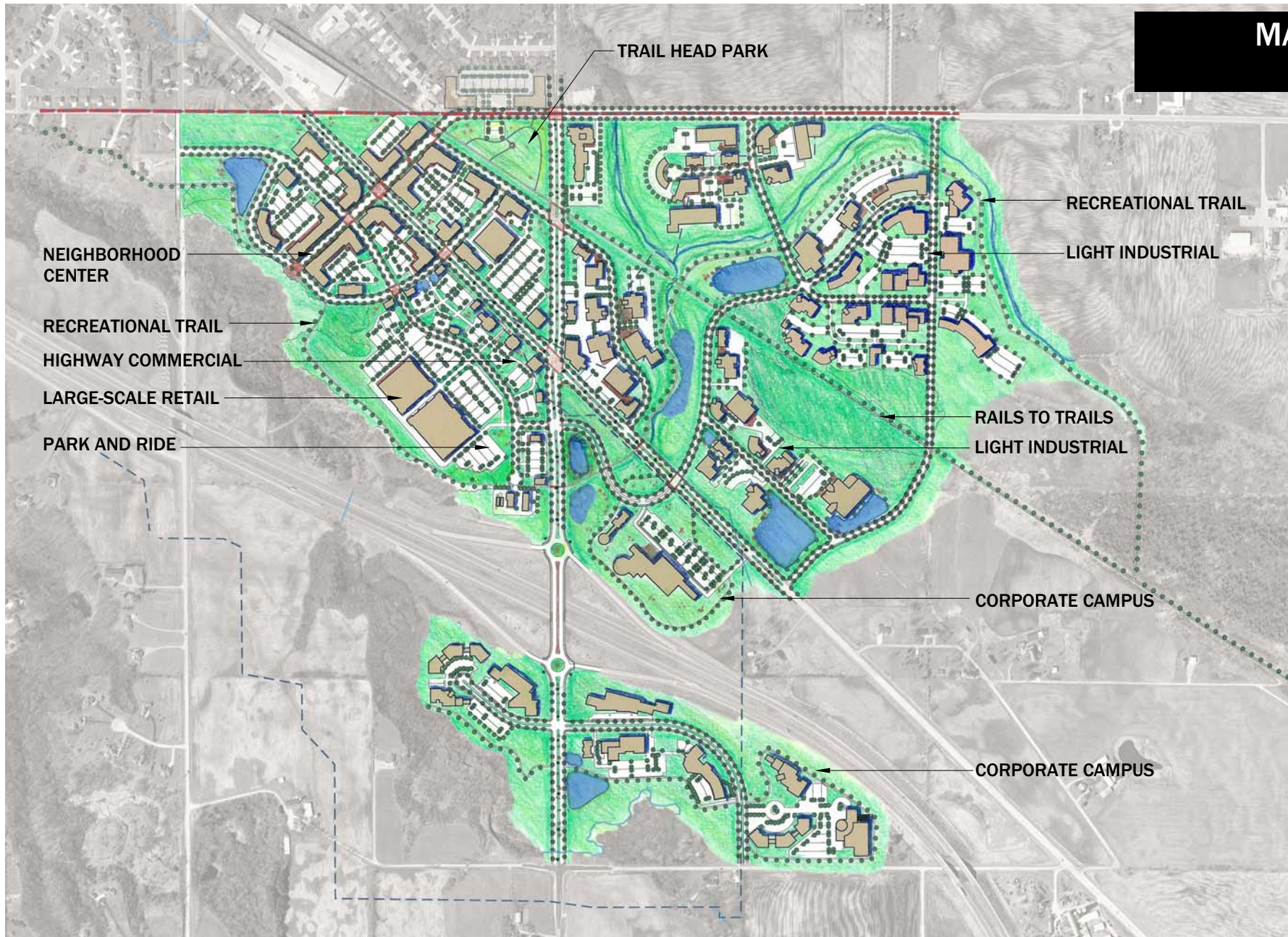
Town of Ledgeview  
Brown County, WI

Project # 2134  
April 20, 2006



### Legend

- Municipal Boundary
- Rivers and Streams
- Sewer Service Area



0 300 600  
SCALE 1"=300'-0"  
(SCALE INDICATED FOR 24"x36" SHEET)

Figure 2





# BUSINESS PARK CHARACTER AREAS

LEDGEVIEW  
BUSINESS PARK

Town of Ledgeview  
Brown County, WI

Project # 2134  
April 20, 2006



SCHREIBER / ANDERSON  
ASSOCIATES, INC.



Figure 3



# Implementation

## *Town Implementation Steps*

Although the Town does not own any land within the business park, it has the central role of enabling, regulating, and approving development projects within the business park. This section outlines the Town's necessary actions to implement the business park.

### *Amend the Comprehensive Plan*

The boundaries of the business park should be changed on the Future Land Use Map of the Town's Comprehensive Plan. Areas that are not connected to the business park and are only suitable for residential uses should be removed. Areas adjacent to the business park and suitable for business park uses should be included. Figure 5: Recommended Comprehensive Plan Amendments depicts areas to be removed and added to the Future Business Park designation.

Through a comprehensive plan amendment, the Town should remove the Future Business Park future land use designation from the areas that are designated for strictly single-family residential uses in the master plan. These locations are disconnected from the remainder of the business park and/or are integrated within existing neighborhoods. Non-residential uses are not appropriate, and these parcels should have residential designations, either Future Neighborhood Residential or Future Higher Density Residential.

The single-family residential areas to be re-designated are described below and in Figure 5.

- The portion bounded by Interstate 43, Glenmore Road, and the ravine.
- The portion bounded by Glenmore Road, the Ledgeview-Bellevue town line, the cross point of US 141/STH 29 and the town line, and the southeast corner of the single residential structure north of the ravine.

Also through a comprehensive plan amendment, the Town should remove the Future Business Park designation from the seven-acre parcel bounded by the town line, Cottage Grove Road, the rail line, and existing residential development. This parcel (combined with the residential parcel to the west) is an ideal location for a future trailhead park along a future rails-to-trails conversion. The trailhead park is a necessary public open space component of the greater business park, but the business park land uses, development guidelines, and infrastructure needs are not applicable. The Town should work to preserve the open space nature of this parcel, particularly before the Eastern Arterial is extended to Interstate 43. (See the Funding Opportunities section for strategies for open space preservation.) When the parcel is under public ownership or the open space has been permanently preserved, the parcel should be rezoned to C-1 (Conservancy District).

Through a comprehensive plan amendment, the Town should add the Future Business Park future land use designation to developed parcels that are adjacent/surrounded by designated Future Business Park areas, the town line, and the ravines, and that are not within the Pedestrian-Scaled Mixed Use area. This re-designation enables redevelopment through the recommended Planned Development District-Business Park (PDD-BP) zoning district and in accordance with the business park development guidelines. Figure 5 shows which areas should receive the Future Business Park designation.

- Truck repair shop in southwest corner of CTH MM and I-43
- Gas station in northwest corner of CTH MM and I-43
- Residential homes along Wall Street and Buddy Lane
- Old salvage yard along rail line east of Wall Street
- Commercial uses on east side of Cottage Road

- Parcels east of Cottage Road, north of designated business park, and south of and adjacent to STH 29

Through a comprehensive plan amendment, the Town should add the Future Neighborhood Center designation to the Pedestrian-Scaled Mixed Use area. Figure 5 shows which area should receive the Future Neighborhood Park designation. Some of this area is currently designated Future Business Park and some is not. This re-designation enables development and redevelopment through the existing Neighborhood Center District (NCD).

Further expansion of the business park boundaries (e.g. further south along CTH MM, further southeast along US 141, and further east along STH 29) are not recommended. Natural features such as ravines and streams provide effective buffers between the business park and planned residential uses.

#### *Amend the Neighborhood Commercial District and Rezone Pedestrian-Scaled Mixed Use District*

The Town regulates the Pedestrian-Scaled Mixed Use District through the existing Neighborhood Center District (NCD). The Town should amend Paragraph A of the Purpose and Intent section of the NCD to allow its use in the Ledgeview Business Park.

Similar to the approach that the Town used in the GV corridor, the Town should administratively rezone the areas designated as the Pedestrian-Scale Mixed Use District to NCD. The Town should consider the existing uses to be legal nonconforming uses. The NCD permitted uses and design standards are appropriate for this area. When implementing the requirements for greenspace and amenities for this district, the Code of Development and Procedure for Approval should be considered for the entire Pedestrian-Scale Mixed Use District and not for each individual NCD parcel.

The development plan and subsequent submittals required as part of the NCD process shall be consistent with the guiding principles and the general recommendations in the Ledgeview Business Park Master Plan as they relate to general land use, urban design, transportation, and utility design.

#### *Create a PDD-BP District*

The balance of the areas with Future Business Park designation that are not regulated through NCD should be regulated through a new version of the Planned Development District (PDD). The Town should create a new PDD zoning district, the Planned Development District-Business Park (PDD-BP). The stated intent of the PDD closely matches the desired outcome for the Ledgeview Business Park:

The purpose of the planned development district and the regulations applicable to the same are to encourage and provide means for effecting desirable and quality development by permitting greater flexibility and design freedom than that permitted under the basic district regulations, and to accomplish a well-balanced, aesthetically satisfying Town and economically desirable development of building sites within a planned development district. These regulations are established to permit latitude in the development of the building site if such development is found to be in accordance with the purpose, spirit and intent of this chapter and is found not to be hazardous, harmful, offensive or otherwise adverse to the environment, property values or the character of the neighborhood or the health, safety and welfare of the community. It is intended to permit and encourage diversification, variation and imagination in the relationship of uses, structures, open spaces and heights of structures for developments conceived and implemented as comprehensive and cohesive unified projects. It is further intended to encourage more rational and economic development with relationship to public services, and to encourage and facilitate preservation of

open space and other natural features, such as woodlands, floodplains and wetlands. (Ledgeview Code, 135-104)

The Town and area developers are already familiar with the basic PDD district and its required submittals and coordination process. A PDD-BP would afford the Town the necessary process to review proposals and developers the zoning flexibility needed to realize the master plan. Unlike conventional zoning districts, the PDD-BP district does not include specific standards for use, setbacks, and intensity. These standards are created through the PDD-BP review process and will be based on the detailed development guidelines in the Ledgeview Business Park Master Plan. The flexible engineering standards allow for more efficient design of streets and parking areas while still meeting public safety needs.

The business park area will be regulated through either the existing Neighborhood Center District (NCD) or the new PDD-BP district, so for ease of understanding and implementation, the two districts should be structured and implemented similarly. The creation and initial implementation of the NCD should guide the creation and initial implementation of the PDD-BP.

The NCD is an independent zoning district (not an overlay) that contains both allowable uses and detailed design guidelines. Thus, the PDD-BP should not be an overlay, but a complete and independent zoning district. Just as the NCD incorporates both allowable uses and specific design regulations, the development guidelines should be incorporated into the new PDD-BP. Since the development guidelines will be embedded within the zoning district, the specific requirements of the district will be clear for landowners and developers. Changes to the development guidelines will require amendments to the zoning ordinance.

During the creation and the implementation of the PDD-BP, the Ledgeview Business Park Development Guidelines will provide guidance to the Town and developers to illustrate the desired physical form of development. The PDD-BP should have requirements for the character and intensity of land use, landscaping of parking areas, engineering design standards, and preservation and maintenance of open space. These base PDD-BP standards must be met for development within the business park.

#### *Permitted Land Uses*

Permitted land uses should be listed within the new PDD-BP, just as permitted land uses are listed in the NCD. The permitted uses should be listed separately according to character area. The PDD-BP should contain a specific list of permitted uses that are consistent with these types of uses.

- **Light Industrial Areas:** The light industrial area should be a mixture of general offices, general business retail, and light industrial uses. Sexually oriented adult entertainment establishments should be neither permitted nor conditional uses through the PDD-BP development plan.
- **Corporate/Support Areas:** The corporate/support area should be a mix of office and light industrial uses within a more campus-like open space setting.
- **Large-Format Retail Area:** The big box area will allow a typical national big box retailer to be located within the business park and easily connected to the interchange. (Note: Outside the Ledgeview Business Park, the Ledgeview Zoning code, particularly the B-3 district, is not well equipped to ensure that large-format retail developments support surrounding land uses and contribute to the Town's visual quality. The Town should consider a town-wide large-format retail ordinance, especially due to likely big box development in the business park as well as in the GV corridor.)
- **Highway-Oriented Commercial Area:** The uses in this area provide services to travelers on the interstate, as well as auto-oriented convenience uses for business park employees and visitors.

#### *Business Park Development Guidelines and Commercial/Multi-Family Design Standards*

The Town adopted in February 2004 stricter design standards for multifamily, business, and industrial developments, found in 135-11(M) of the Ledgeview Code. (The affected uses are those within the following

districts: PDD Planned Development District, I-1 Industrial, B-1 Neighborhood Business, B-2 General Business, LI Light Industrial, HI Heavy Industrial and R-3 Multiple Family Districts.) The design standards cover building exterior materials, fencing, lighting, landscaping, off-street parking and loading, outdoor storage, and height and yard requirements.

All business park buildings in developments implemented through the PDD-BP should comply with these commercial standards, and 135-11(M) should be amended to include PDD-BP if the new zoning district is created. In limited cases, the business park development guidelines have stricter standards, and these stricter standards should be incorporated into the PDD-BP. These design standards should be included in the PDD-BP development plan.

- 40% of open space is required for the Corporate/Support District (Section 135-11(M)(10)(e) requires only 25% for PDD.)
- The following stricter standards for Off-Street Parking Requirements (135-11(N)) should be included in the PDD-BP development plan.
  - (1) Location. In all business park areas, off-street parking should not be located between the street and the building front. Off-street parking should be permitted to be located on a different zoning lot, as long as it is located within 600' of the primary entrance and a shared parking agreement is approved by the Town and recorded.
  - (11) Surfacing. Pervious surfaces should be permitted for a portion of the parking areas where appropriate.
  - (16) Parking lot landscaping. In the corporate/support areas, the parking lot landscaping regulations should require more islands and plantings and provide clearly delineated walking paths across parking areas and drive aisles, as described in the development guidelines.
- The PDD-BP development plan should require that all ground signs be monument signs.

#### *Review Procedures for PDD-BP*

The NCD and PDD processes do not require any additional public involvement beyond a public hearing. The Town could consider the public meetings conducted by SAA during the preparation of the master plan to be sufficient public involvement. However, should the Town wish to further receive community input regarding specific developments within the business park during the development process, the Town should add necessary public meeting requirements to the NCD and PDD-BP districts before administratively rezoning parcels to NCD or PDD-BP. The Town should both host and lead the public meetings.

Town staff is familiar with the NCD and PDD and has had experience in executing and regulating PDD development plans. The Town does not have a full time staff planner. It is recommended that the Town retain the services of a professional planner experienced with the PDD and PDD-BP process and familiar with planning and urban design issues to assist through the development review process. County planners or on-call planning consultant may perform this function. When developers approach the Town, the Town's staff planner should work proactively with the developer and their design team to communicate the Town's priorities and address any concerns or potential conflicts early in the process.

#### *Initial Implementation of the PDD-BP District*

For ease of understanding and implementation, the initial implementation of the NCD should guide the initial implementation of the PDD-BP. After the adoption of the NCD, the Town administratively rezoned the areas along the GV corridor that were designated Future Neighborhood Center on the Future Land Use Map of the Comprehensive Plan. The Town now considers the corridor's existing uses to be nonconforming uses and existing buildings to be nonconforming buildings. Hence, the existing uses and buildings can remain and function as they currently do.



However, parcel owners will need to comply with regulations of the NCD district when substantial changes are made to the building or land use. The Town's existing regulations on nonconforming uses describe the conditions when nonconforming uses must be changed to be conforming. Section 135-9 of the Ledgeview Code reads, in part:

G. The existing lawful use of a building or premises at the time of the enactment of this chapter or any amendment thereto may be continued, although such use does not conform with the provisions of this chapter for the district in which it is located, but such nonconforming use shall not be extended nor shall such building or structure be altered or enlarged, except as provided for in Subsection J.

H. If no structural alterations are made, a nonconforming use of a building may be changed to another nonconforming use of the same or a more restricted classification. Whenever a nonconforming use has been changed to a more restricted nonconforming use or a conforming use, such use shall not thereafter be changed to a less restricted use.

I. If a nonconforming use of a building or premises is discontinued for a period of 12 months, any future use of the building or premises shall conform to the regulations for the district in which it is located. Existing farming operations shall be exempt from these restrictions.

J. When a building containing a nonconforming use is damaged by fire, explosion, act of God or the public enemy to the extent of more than 50% of its current local assessed value, it definitely shall not be restored, except in conformity with the regulations of the district in which it is located. Total structural repairs or alterations in any nonconforming use shall not, during its life, exceed 50% of the local assessed value of the building at the time of its becoming a nonconforming use, unless permanently changed to a conforming use.

Section 135-8 of the Ledgeview Code provides the following definitions:

**NONCONFORMING BUILDING** — A building lawfully erected at the time of the enactment of this chapter which does not conform to the height, setback, yard, parking or other bulk requirements of this chapter or any amendment thereto governing the zoning district in which such building is located.

**NONCONFORMING USE** — Any use of land, buildings or structures lawful at the time of the enactment of this chapter which does not comply with all of the regulations of this chapter or of any amendment hereto governing use for the zoning district in which such use is located.

In a similar fashion, the Town should administratively rezone the portions of the business park that are not designated Pedestrian-Scale Mixed Use District. The Town should consider the existing uses and buildings within the business park area to be nonconforming uses and buildings, and the Town should require conformity with the PDD-BP when the Town's nonconforming use triggers are met.

#### *Alternate Implementation Approaches*

The Town has two other options to incorporate the development guidelines into the review process. These approaches are not recommended, but may be considered if the Town does not wish to create a new PDD-BP district. The other options are:

- The Town can adopt an overlay for the Future Business Park designated parcels. Landowners and developers would need to refer to both the underlying zoning district regulations as well as the additional overlay regulations.



- The Town Board could endorse the development guidelines by just adding a reference to the development guidelines within the adopted PDD. For example, the Town could insert a new section: 135-108(G) “When the parcel has a future land use designation of Future Business Park, the land use design, streets, streetscapes, and open spaces shall support and further the intents and guidelines within the Ledgeview Business Park Development Guidelines.” The guidelines would be adopted by reference, and later changes to the development guidelines would not require amendments to the zoning code.

### *Create an Official Map*

The Town should create and adopt by ordinance an Official Map that includes the proposed public street network and public open spaces as shown in the master plan. An official map is a tool to reserve the location for street rights-of-way, infrastructure, parks, and other planned public improvements.

Developers/landowners would necessarily need to construct the shown public improvements (with public assistance or reimbursement) or preserve the right-of-way (for later public construction).

The Official Map should show the internal street network of the business park. The street network shown in the master plan enables efficient development, protects the natural environment, and maintains safe traffic flows. As development plans are brought to the Town, improvements on the street and other infrastructure networks are encouraged. However, the street network shall achieve the following principles:

- Full intersections: To minimize the number of intersections on regional roadways while still providing maximum accessibility, intersections should be full, four-way intersections. New road connections should convert existing T-intersections to full intersections, including at Main Street/Kewaunee Road, CTH MM/Cedar Hedge, and CTH MM and the existing entrance to the truck repair shop south of the I-43 interchange. New intersections should also be full intersections, as much as practical.
- Interconnected character areas: New internal roadways, in addition to existing roadways, should connect the adjacent character areas. A continuous street should connect the Big Box and Pedestrian-Scale Mixed Use areas. Additionally, a continuous street should connect the Box Box area, the Corporate/Support area north of I-43 with the Light Industrial area south of the rail line, the Light Industrial area north of the rail line, and STH 29. Continuous streets are not necessarily direct streets; they do permit vehicular passage through the area without undue out-of-way routing.
- Connected to adjacent areas: Except where blocked by environmental corridors, the street network shall connect to the adjacent streets. The new internal street network should connect with Shadow Lane, Glenmore Road, and Kewaunee Road.
- Environmental impacts minimized: More than one connection between the Light Industrial areas north and south of the rail line/bike path is necessary. These connections shall be accomplished within minimized environmental impacts.
- Connection to Village of Bellevue Commercial Park: The street network in the Light Industrial area south of STH 29 should enable full intersections with roads that support the planned Village of Bellevue Commercial Park. The interconnected road systems will integrate the supportive land use of each business park and also improve relative safety for motorists of STH 29.
- Public edge to ravines: In the Pedestrian-Scale Mixed Use area, a road should run adjacent to at least a portion of the ravine area to give the ravine a “public edge.” A public edge allows the general public a direct connection with the ravines, rather than the natural area hidden behind buildings and private areas.

The Official Map may also show the location of bike trails. As development plans are brought to the Town, improvements on the bike trail networks are encouraged. However, the bike trail network should achieve the following principles:

- Public/private trail network: Trails within the corporate/support area are primarily for private use, but they should connect with planned bike lanes on CTH MM. The remainder of the trail network,

including the trail through the corporate/support area that connects CTH MM and US 141, should be public.

- Public edges for natural features: Bike trails should run adjacent to natural areas such as the ravines west of CTH MM and north of I-43, and the stream north of Main Street and east of CTH EA.
- Frequent trail intersections: Trails should interconnect with the planned bike lanes on CTH MM and the proposed rails-to-trails conversion.

The Area Development Plans created through the NCD and PDD-BP process should be included on the Official Map, and existing Area Development Plans within the business park area should be modified to conform to the master plan.

### *Forge Partnerships*

To ensure a well-designed business park that is integrated into the local community and the region, the Town must partner with surrounding communities on multiple projects.

- Village of Bellevue: The Town of Ledgeview Business Park and the Village of Bellevue existing neighborhood and planned commercial park can be mutually supportive and their individual successes are intertwined. The Town and Village should coordinate development efforts to connect the business park's greenways and trails to local and regional environmental corridors and trail facilities, to coordinate the connections between the business park and Bellevue's planned commercial park, and to coordinate the design and continuity of the Eastern Arterial. The Town and Village should coordinate regarding development along their common boundary, including supportive or common design standards along the boundary to create an attractive and consistent STH 29 corridor.
- Brown County: County planning staff can serve as advisers for implementation of the business park. County economic development officials can assist in targeting developers. The Highway Department will plan and construct the Eastern Arterial extension, as well as the other state and county routes within and adjacent to the business park.
- Wisconsin Department of Transportation: Coordination with WisDOT is necessary to jointly plan and design the ramps for Interstate 43, including roundabouts at the ramp connections to CTH MM, and landscaping and signage within the roundabouts.
- Department of Natural Resources and Department of Transportation: At the point when the rail line is no longer in use, the DNR, DOT, and the county can be funding partners in converting it to a regional bike trail.
- Fox Valley and Western Railroad: While the rail line is in operation, the Town will need to negotiate additional road crossings for local streets (in addition to the Eastern Arterial crossing to be negotiated by the County).

### *Estimated Cost of Improvements*

With the exception of the extension of water and sewer mains that are a part of the Ledgeview Northerly Interceptor Phase 3 Project, the Town intends that the Town will design and developers will fund most of the additional infrastructure necessary for the business park.

The Town will design, own, and maintain all streets within the business park, since all streets serve multiple parcels or are necessary for interconnection. Additionally, the Town will design, own, and maintain trails that connect planned regional trail systems. Either the developer will build the necessary street and trail sections, or the Town will build the street and trail sections and will be reimbursed by landowners/developers. The costs for design and construction of streets and public trails will be paid upfront by developers and/or assessed to properties served. Maintenance costs will be borne by the Town. The Town Board will arrange the timing and method of reimbursement, when appropriate.

Trails in the Corporate/Support area south of I-43 do not connect regional systems and are completely internal to single property owners, so these trails will be privately designed, built, owned, and maintained.

The Town will construct necessary regional stormwater ponds and will assess the costs to properties served. The Town will appraise and purchase land required for regional ponds, and the Town will include the cost of the property in the assessment to each parcel served by the pond. Regional stormwater ponds should be constructed when any parcel within the pond's sub-watershed is developed. The Town may construct ponds prior to development, if grants or financing is available.

The Town will design, own, and maintain the public business park amenities and the trailhead park. Either the developer will build the amenities, or the Town will build the amenities and will be reimbursed by landowners/developers. The public amenities cost estimate includes costs for entry monuments, wayfinding signs, bike crossing signals, a landscape allowance, and site amenities (benches, trash receptacles, bike racks, etc.). The trailhead park cost estimate considers the public improvements necessary for the small triangular park at the Eastern Arterial and town line. The estimate includes design and construction of the entire park (both the parcel currently designated Future Business Park and the residential parcel to the west). The cost estimate considers neither the rails-to-trails conversion nor acquisition cost for the parcels designated for the park. At its option, the Town may assess these costs evenly to parcels within the business park.

The master plan assumes that there will be improvements to the existing regional roadways. It is assumed that the Wisconsin Department of Transportation and/or Brown County will fund the majority of reconstruction of these roadways to their typical design standards. The Town will be responsible to partner with WisDOT and the County to upgrade the roadway design to the standards included in the master plan.

- *Eastern Arterial:* The cost estimate assumes the Town will be required to pay a portion (assumed to be 20 percent) of the curb-to-curb improvements between STH 29 and the I-43 interchange, with the majority of the cost funded by Brown County and the Wisconsin Department of Transportation. The streetscape elements and the entry features within the roundabouts are beyond the minimum WisDOT requirements, and thus the Town funds those costs.
- *Regional Arterials:* Improvements to US 141 and STH 29 are also funded primarily through the county and WisDOT, with the Town paying for a portion (assumed 20 percent) of improvements. These costs estimates include only the sections of those roads within the business park area.
- *Former STH 29:* After the extension of the Eastern Arterial to the interstate interchange, STH 29 will be redesignated to follow CTH EA. The jurisdiction of the former STH 29 right-of-way (between Eastern Arterial and US 141) will likely be converted to the Town. It is common WisDOT practice to rebuild the street cross-section and streetscaping to the local municipality's adopted guidelines before transferring the jurisdiction to a city, village, or town. The Town will need to partner with WisDOT during the reconstruction and redesignation of this portion of STH 29.

The costs contained in Table 1: Summary of Estimated Costs by Phase are estimates for all typical street cross-sections and streetscapes, the public open space, and amenities throughout the business park. Full cost estimates, broken down by street type, are included in the Appendix. Maintenance costs have not been estimated.

*Table 1: Summary of Estimated Costs by Phase*

	<i>Phase I</i>	<i>Phase II</i>	<i>Phase III</i>
Internal Streets			
Light Industrial/Big Box	\$2,502,597		\$6,435,251
Village Center Type 1			\$2,954,424
Village Center Type 2	\$532,795		\$5,387,151
Corporate		\$2,430,589	
<i>Streets and Streetscaping Subtotal</i>	<i>\$3,035,392</i>	<i>\$2,430,589</i>	<i>\$14,776,826</i>
Amenities			
Entry Monument	\$40,000	\$40,000	\$80,000
Wayfinding Signs	\$5,000	\$5,000	\$5,000
Bike Crossing Signals		\$36,000	
Pathways - 10' wide, asphalt	\$55,800	\$119,970	\$103,230
Shade Trees	\$19,300	\$41,495	\$35,705
Bike Bridge			\$100,000
Site Amenities	\$6,000	\$12,900	\$11,100
Landscape Allowance	\$6,500	\$6,500	\$66,500
35% Contingency	\$46,410	\$91,653	\$140,537
<i>Amenities Subtotal</i>	<i>\$179,010</i>	<i>\$353,518</i>	<i>\$542,072</i>
Trailhead Park			\$422,291
Eastern Arterial*	\$2,145,760		
Regional Arterial*	\$2,448,919		
	<b>\$7,809,081</b>	<b>\$2,784,107</b>	<b>\$15,741,189</b>

See the Appendix for a full description of assumptions and included elements for each budget line.

\* These costs are estimates for the Town's portion of the State/County roadway project (assumption – 20%), plus full costs of streetscaping.

## ***Funding Opportunities***

The Town has a number of opportunities for funding both public improvements and assisting in funding private improvements. The Town should investigate each of the following, specifically considering eligibility requirements for each mechanism/program.

*Community Development Block Grant Public Facilities for Economic Development (Wisconsin Department of Commerce):* The CDBG-PFED Program helps underwrite the cost of municipal infrastructure necessary for business development that retains or creates employment opportunities. Eligible activities are improvements to public facilities such as water systems, sewerage systems, and roads that are owned by a general or special purpose unit of government, which will principally benefit businesses and as a result will induce businesses to create jobs and invest in the community. As a general purpose unit of government with a population less than 50,000, and the Town is eligible. Each project is eligible for up to \$750,000 in CDBG-PFED funds per calendar year.

*Community Development Block Grant-Economic Development (Wisconsin Department of Commerce):* The CDBG-ED program is designed to assist businesses that are willing to invest private funds and create jobs as they expand or relocate to Wisconsin. The Wisconsin Department of Commerce would award the funds to the Town, which then loans the funds to a business. When the business repays the loan, the Town can retain the funds to capitalize a local revolving loan fund (see below). See the Wisconsin Department of Commerce for eligibility requirements.

*Community-Based Economic Development Program Development (Wisconsin Department of Commerce):* The CBED Program provides financing assistance to local governments that undertake planning or development projects or that provide technical assistance services that are in support of business and community development.

*Brown County Economic Development Revolving Loan Fund:* The County acts as a bank and gives low-interest loans to small businesses. In turn, these businesses bring in new investment, increase tax base, and create jobs in the region. The County can then utilize repayments for loans to assist other businesses that further create employment opportunities and increase local incomes.

*Revolving Loan Fund:* The Town could create a revolving loan fund to assist businesses looking to expand within the Town. Money for the revolving loan fund can come from a CBED grant and the CDBG-ED Program (which provides matching grants to start or re-capitalize a revolving loan fund, up to \$50,000/year). TIF revenues can be used to fund private redevelopment activities such as acquisition, demolition, site preparation, and other redevelopment activities deemed appropriate. Typically, there is a maximum loan amount and term, and business and property owners must provide matching funds.

*Industrial Revenue Bond Program (Wisconsin Department of Commerce):* This program allows Wisconsin cities, villages, and towns to support industrial development through the sale of tax-exempt bonds. The proceeds from the bond sale are loaned to businesses to finance capital investment projects at, primarily, manufacturing facilities. Even though Industrial Revenue Bonds are municipal bonds, they are not general obligations of the municipality. The company or business that will use the facility provides the interest and principal payments on the loan. The Town is in partnership with the business, lending its name, but not its credit, to the bond issue. This program can be applicable to the manufacturing facilities within the light industrial area of the business park. Manufacturing generally includes nearly every type of processing that results in a change in the condition of tangible property. The proceeds of the bond issue may be used to finance the cost of land, construction of new or expanded facilities, purchase of equipment, and the payment of certain costs incurred in the issuance of the bonds.

*Local Transportation Enhancement Program (WisDOT):* This program is designed to fund projects that enhance multi-modal activities in order to improve the transportation system. Projects in the business park which may

qualify for funding include the conversion of the railway corridor to a public trail, a wide variety of projects such as bicycle or pedestrian facilities, acquisition of scenic easements along the ravine edge for trails, and landscaping. Currently \$6.25 million is available in annual funding, and 80 percent of costs are funded through federal funds. Construction projects should total at least \$100,000 and non-construction projects at least \$25,000 in order to be eligible.

*Transportation Economic Assistance Grants (WisDOT):* Transportation Economic Assistance (TEA) grants help communities pay for transportation improvements that will help attract and retain employers. TEA grants may bypass normal state scheduling to promote economic development projects that are dependent on transportation improvements. The maximum grant award is \$1 million per project. The TEA grants may fund up to 50% of project costs, and the local match may come from any combination of local, federal, or private funds or in-kind services.

*State Infrastructure Bank Program (WisDOT):* The State Infrastructure Bank (SIB) program, similar to a private bank, offers a range of loans and credit options to help finance eligible surface transportation projects. SIBs offer states the ability to undertake transportation projects that would otherwise go unfunded or experience substantial delays. Communities can borrow money to provide needed transportation infrastructure improvements to help preserve, promote and encourage economic development and/or promote transportation efficiency, safety and mobility. The Wisconsin SIB program is a revolving loan program providing capital for transportation projects from loan repayments and interest earned from funds remaining in the bank. The program currently has \$700,000 available for loans to eligible applicants. WisDOT charges 2% interest rate on the loan principal and projects can be amortized up to 25 years.

*Brownfield Economic Development Initiative (BEDI) Grants (US HUD):* The Brownfield Economic Development Initiative (BEDI) provides eligible communities with grants to clean up and redevelop brownfields. The program, run by the U.S. Department of Housing and Urban Development gives out a maximum grant award of \$2 million. This can be used for planning, property acquisition, phase I and II environmental site assessments, environmental cleanup, redevelopment, marketing, and public facility and infrastructure improvements.

*Brownfields Site Cleanup Grant (US EPA):* The later phases of the Ledgeview business park will require clean up of industrial sites. These grants are administered by the U.S. Environmental Protection Agency (EPA) for the cleanup of a specific brownfield site that is identified in the application. These grants provide up to \$200,000 per site for cleanup.

*Wisconsin Business Development Finance Corporation:* The Wisconsin Business Development Finance Corporation is a private, not-for-profit corporation which assists Wisconsin businesses gain access to capital, create jobs and promote community development. The organization provides low-interest loans, long term financing, and flexible payment options to small businesses looking to expand or new businesses willing to locate in the region, purchase of machinery and equipment and furniture. This tool would attract smaller businesses into the region, reducing their initial start up costs and providing them with incentives to stay in the region.

*Great Lakes Asset Corporation (GLAC):* The Great Lakes Asset Corporation is a private, non-profit development corporation created to serve the long-term credit needs of small businesses. GLAC is the local contact for several SBA loan programs. The industrial facilities within the business park would benefit from this program. GLAC participates with financial institutions on the package, which may consist of the purchase of land, building, machinery, and equipment and/or new construction. The organization works with several counties in Wisconsin, including Brown County.

*Bay Area Regional Planning Commission:* Grants are available through the Bay Area Regional Planning Commission, which works together with 14 counties as part of the North East Wisconsin Regional Economic Partnership (NEWREP). NEWREP assists businesses that use advanced technology in their production, operations, or manufacturing processes.

*Open Space Funding Sources:* Although it is recommended that the parcel shown as open space at the southwest corner of Cottage Road and STH 29 be removed from the Future Business Park designation, the acquisition of the parcel is still important to the business park development. State and federal governments provide money to local governments for the acquisition and development of parks. It is important to note that most of the programs require that a local government submit to the Wisconsin Department of Natural Resources a comprehensive outdoor recreation plan or master plan which has been approved by resolution by the submitting local government.

This section has general introductory information for many of the grant programs that may be used to acquire and develop local park facilities in Wisconsin. The Town should investigate the eligibility of projects and applicants for each grant program.

- **Stewardship Program (WDNR):** This program contains funds for state and local governments, as well as nonprofit conservation organizations. To encourage partnerships and cooperation between the WDNR, local governments, and conservation organizations, the Stewardship Program provides a matching grant that is available to non-profit organizations for the acquisition of land or land rights for a variety of land conservation and recreational purposes. The Stewardship Program funds these projects:
  - Acquisition and Development of Local Parks, 50% match
  - Urban Rivers, 50% match
  - Urban Greenspace, 50% match
  - Acquisition of Development Rights, 50% match
- **Recreational Trails Program (WDNR):** Funding is provided through federal gas excise taxes paid on fuel used by off-highway vehicles. Towns and other jurisdictions are eligible to receive reimbursement for development and maintenance of recreational trails and trail-related facilities for both motorized and non-motorized recreational trail uses. Eligible sponsors may be reimbursed for up to 50 percent of the total project costs. Eligible projects include acquisition of easement or property for trails, development and rehabilitation of trailside and trailhead facilities and trail linkages, construction of new trails maintenance, and restoration of existing trails.
- **Urban Forestry Grants (WDNR):** Communities interested in developing urban forestry programs can receive financial assistance. Towns and other jurisdictions can receive awards ranging from a minimum of \$1,000 to a maximum of \$25,000. Fifty percent cost sharing of eligible project costs for approved practices is required. Eligible projects include undertaking street tree inventories, developing urban open space programs, developing urban forestry plans, and tree planting and maintenance.
- **Federal Programs:** Federal funding is available for park acquisition through programs including the Land and Water Conservation Fund and the Recreational Trails Act.

*Special Assessments:* This financing tool allows the local government to use special assessments to help pay for local improvements based on the benefit the improvement gives the property. Special assessments can be used for a variety of services provided. Some of these include:

- Streets, sidewalks, pavement, gutters, curbs, vehicle parking strips, grading, graveling, beautification, and storm sewers or other street drainage and connections from sewer, water, or similar mains to curb lines. It also includes curb, gutter, and storm sewer on town, county, and state roads.
- Storm and sanitary sewers and systems, including outlets, holding areas and ponds, treatment plants, pumps, lift stations, service connections, etc.
- Street lights, street lighting systems, and special lighting systems



- Water works systems, including mains, valves, hydrants, service connections, wells, pumps, reservoirs, tanks, treatment plants, etc.
- Parks, open space areas, playgrounds, and recreational facilities
- Planting trees on streets and providing for their trimming, care, and removal
- Abatement of nuisances, draining and filling swamps, marshes, and ponds on public or private property

*Tax Increment Financing:* If the Town is successful in its incorporation effort, it could create a tax incremental district to fund utility and road construction and street improvements within the business park (or any TIF district that the incorporated municipality defines). Wisconsin law allows cities and villages to create tax incremental districts, but restricts Towns without a cooperative boundary agreement from creating a tax incremental district. It is recommended that the Town consult with an economic development/TIF expert before establishing the district.

## ***Development Phases***

The implementation of the business park has been divided into three phases. Each phase provides a sequential ordering of improvements and is based on market demand and parcel location. Although phases may overlap, it is recommended that the Town concentrate its efforts on each phase sequentially. See Figure 6: Phasing Plan for a depiction of recommended phasing.

### ***Phase I***

Phase I will set the bar for design standards and the pattern of development for all phases of the business park. The timeframe for the completion of Phase I of the business park is 2-5 years. Private investment reflecting current rural zoning and character is now happening and will continue to happen at a gradual pace until basic public investments are constructed. Sanitary sewer and water facilities will be extended to the business park area by mid 2007.

The criteria that determined which areas have been included in Phase I are:

- *Market:* Phase I includes land uses that have immediate demand in the area. The existing highway-oriented retail will be expanded along CTH MM, large format retail with outlots west of CTH MM, and two disconnected areas of light industrial uses along the north side of US 141 and south of STH 29.
- *Location:* The sites are located along existing major travel corridors (CTH MM, US 141, STH 29) and require no immediate regional roadway improvement for basic transportation access.

Implementation challenges specific to Phase I:

- *Connections to utilities:* Planned sewer and water mains along the spines of the business park areas will be completed by mid 2007. Phase I development areas are adjacent and can connect to existing and planned sanitary sewer and water mains.
- *Construction of roadways/trails:*
  - The light industrial area north of US 141 and south of the rail line will need to construct one road with a connection to US 141 to provide access to the site. In Phase I, this road should dead-end before the rail line, to be extended north in Phase III. The internal roadway should be designed to allow a connection to an additional roadway at the eastern edge of this area, to be constructed in Phase III.
  - The light industrial area south of STH 29 and north of the rail line currently has access off STH 29, serving the mini-warehousing structures. The access road will need improvement to meet the roadway design standards when additional Phase I light industrial is constructed. The roadway will be a two lane undivided road with 60' right-of-way and sidewalks on both sides wherever required.
  - Cedar Hedge Lane now provides access from CTH MM to the highway-oriented convenience/gas station. This intersection will also serve the additional highway-oriented development, large format retail, and the outlot commercial uses. When Cedar Hedge Lane is redirected and reconstructed, the connection to CTH MM and internal roadway should be a 60' right-of-way. Phase I also includes a roadway connection and new intersection on US 141 to provide two access points to the large format retail area.
- *Construction of storm water ponds:* There are five sub-watersheds in the business park area, and Phase I development occurs in three of them, so Phase I requires the construction of three regional stormwater ponds.
  - Pond that is approximately 400' east of Glenmore and approximately 400' south of town line (within sub-watershed #23 in the regional stormwater plan): This pond serves the sub-watershed

- that contains the Phase I large-format retail building, outlots, and parking. This pond will also serve the pedestrian-scale mixed-use area that is part of Phase III.
  - Pond located north of US 141 approximately 800' northwest of Shadow Court (within sub-watershed #25): This large pond is shown on the master plan as two smaller ponds, but the ultimate design is up to the landowner. These ponds will serve the light industrial facilities to be constructed north of US 141 and south of the rail line.
  - Pond located north of and adjacent to the rail line, approximately 1000' south of STH 29, and approximately 1000' east of Cottage Road/CTH EA (within sub-watershed #26): This pond serves light industrial development south of STH 29. This pond will also serve additional light industrial development in Phase III.
- *Land Assembly/Landowner Coordination:* In Phase I, no land assembly is necessary. Coordination between adjacent landowners is necessary in two locations: providing access to STH 29 to the northeast light industrial area, and coordination of access and the internal roadway in the big box/outlot area, enabling connection of the new internal roadway to connect to a Main Street connection in Phase III.
- *Regional Coordination:*
  - The Town needs to coordinate with the Village of Bellevue regarding the design of the Eastern Arterial. Design and reconstruction of CTH MM between STH 29 and the I-43 interchange will require collaboration between the Town, Brown County, and Wisconsin Department of Transportation. The proposed off-ramps include two roundabouts that will require a feasibility study by WisDOT. It is expected that the Town will contribute a portion of the amount needed to construct CTH EA as per proposed standards.
  - The Town will need to coordinate with Brown County and WisDOT about the extension of the Eastern Arterial cross-section to Shadow Lane, perhaps in conjunction with the corporate/support development in Phase II.
  - Within the light industrial area, a proposed trail bridge crossing the stream between the railroad and US 141 east of Wall Street will need WDNR coordination and approval, which might include mitigation and impact fees.

## Phase II

Phase II will introduce a new land use -- corporate commercial with supporting light industrial uses. Located within the most strategic viewsheds from the interstate, the corporate/support uses will define the entrance to the business park, Ledgeview, and the Green Bay region. Phase II is to be implemented over 5 years.

The criteria that determined which areas have been included in Phase II are:

- *Market:* Regionally, there is a softer demand for corporate office with supporting light industrial. In Phase II, the business park will become more attractive to potential corporate/support users after the Eastern Arterial connection is complete (creating quicker and multiple routes to the I-43 Business Park), the supporting restaurants of Phase I, and the established identity and design character of the interchange area.
- *Location:* As mentioned above, the sites earmarked for Phase II will help define the entrance to the business park, and in the process further its marketability.

Implementation challenges specific to Phase II:

- *Connections to utilities:* Parcels south of the interchange and north of Shadow Lane will have public sanitary sewer mains within the site. Public water mains run close to these parcels along Shadow Lane, therefore providing convenient connection points. The parcels north of the interchange can connect directly to sewer and water mains running along CTH MM.

- *Construction of roadways/trails:*
  - North of the interchange, the Cedar Hedge Lane intersection (expanded to become the entrance to the big box retail development in Phase I), is made a four-way intersection with an entrance to the corporate/support area to the east, crossing US 141, and connecting to the light industrial road built north of US 141 in Phase I. The 80' right-of-way includes sidewalks on both sides of the road, on-street bike lanes, and a 14' landscaped median. The wide median allows for a signature entrance into the corporate/support campus.
  - The trail system on the corporate/support parcels is primarily a private trail system for employee and visitor use. One section of trail is public; it connects CTH MM and US 141, connecting across US 141 to the trail constructed in Phase I in the light industrial area.
  - South of the interchange is a T-intersection providing access off CTH MM to a truck maintenance facility. When the used car lot redevelops as a corporate/support use, the existing roadway can be improved at the time or can be improved in conjunction with a future redevelopment of the truck maintenance facility. East of CTH MM, the intersection is expanded into a four-way intersection. This road connects both parcels east of CTH MM and connects to Shadow Lane. On each corporate/support parcel, the developer is to construct private trails, mainly for employee recreation. The private trails do have the potential to connect south of the interchange under the CTH MM bridge.
- *Construction of storm water ponds:* Phase II development occurs in two sub-watersheds, and two ponds are necessary. Due to the highly visible location of both ponds along CTH MM, it is essential to integrate them within the corporate/support campus. To blend the detention ponds with the corporate/support landscape, trails and entrance signs should be placed such that they frame the ponds.
  - Pond located east and adjacent to CTH MM and south of I-43 ramps (within sub-watershed #21 in the regional stormwater plan): This pond exclusively serves the corporate/support development south of Interstate 43.
  - Pond located east and adjacent to CTH MM and north of I-43 (within sub-watershed #24): This pond will serve the corporate/support development north of Interstate 43. Additional ponds are necessary within the sub-watershed to support redevelopment, and is addressed in Phase III.
- *Land Assembly/Landowner Coordination:* In Phase II, no land assembly is necessary south of the interchange, but the landowners east of CTH MM will need to coordinate on the design, location, and construction of the internal roadway connection near the connection with Shadow Lane. North of the interchange, the landowners will need to closely coordinate. The triangular parcel southeast of the CTH MM/US 141 intersection is designated for a regional stormwater pond and access onto CTH MM, both necessary for the large parcel to the southeast.
- *Regional Coordination:* If not accomplished in Phase I, the Town must coordinate with the County and WisDOT regarding extension of the Eastern Arterial cross-section south of the interchange to Shadow Lane.

### *Phase III*

Phase III of development of the business park is a major expansion of light industrial and a characteristic pedestrian-scale commercial and mixed use development. Phase III is to be implemented over 5-10 years. Phase III will link different uses within the business park using public trails and roads, in the process helping to create and complete the theme of the business park.

The criteria that determined which areas have been included in Phase III are:

- *Market:* Phase III light industrial will respond to continuing market need for that land use at this regionally strategic interchange. The pedestrian-scale commercial and mixed use area will be supported by

the corporate/support workers in Phase II, and continuing residential development in Bellevue and Ledgeview.

- *Location:* Phase III areas are further from the Eastern Arterial, allowing orderly development and utility connection extension.

Implementation challenges specific to Phase III:

- *Construction of roadways/trails:*
  - The new light industrial area south of STH 29 and north of the rail line requires roadway connections at four points. The Phase I road in the area to the northwest should be extended to cross the stream, and the Phase I road to the south should be extended across the old salvage yard and rail line (or rails-to-trails conversion). Phase III also includes the construction of roadway that direct connects STH 29 and US 141 at the eastern edge of light industrial development in Phases I and III. The new roadway should connect with the internal roadway constructed in Phase I in the area south of the rail line. It is recommended that urban residential (either single or multi-family) should be planned on the east side of the roadway to designate the park's eastern boundary and transition to existing residential on Shadow Court.
  - In the pedestrian-scale mixed-use area, access is initially provided through two locations: a connection to the internal road network of the big box/outlot area with a western extension to Glenmore Road, and direct access to US 141 through a Phase I roadway connection between the outlots and the pedestrian-scale mixed use area. When redevelopment of existing parcels occurs at the current STH 29/US 141 intersection, the T-intersection can be expanded to a four-way intersection, allowing a direct connection between the small-scale commercial and mixed use area and the Eastern Arterial along the former STH 29 right-of-way (converted to a Town road).
  - In the light industrial area, trails should be constructed along stream edges and connected to the rails-to-trails conversion (if it has occurred by Phase III). In the big box development area, the trails constructed in Phase I should be extended along the ravine edge through the pedestrian-scale mixed-use area to connect with Glenmore Road.
- *Construction of storm water ponds:* Phase III development occurs in three sub-watersheds, but regional stormwater ponds that serve Phase III development are to be constructed in previous phases. The additional light industrial facilities south of STH 29 and pedestrian-scale mixed-use area south of Main Street are served by ponds constructed in Phase I. One additional regional stormwater pond is necessary.
  - Pond located approximately 800' east of Cottage Road and between US 141 and the rail line (within sub-watershed #24): This pond is a second pond within this sub-watershed, the first constructed in Phase II within the corporate/support area north of I-43. This additional pond enables the highway-oriented and light industrial redevelopment east of CTH EA, and the pedestrian-scaled mixed-use redevelopment west of CTH EA and north of Main Street.
- *Land Assembly/Landowner Coordination:*
  - The new internal roadways within the light industrial area generally follow parcel lines, so land assembly is not necessary. However, coordination among landowners on the design, construction, and timing of the roads is necessary.
  - The greenfield portion of the pedestrian-scale mixed-use area is currently a single owner, requiring no land assembly. However, the remainder of the pedestrian-scale mixed use area is divided among many smaller property owners. Coordination among these landowners is necessary for the roadway connection to the big box/outlot area, the connection to the former STH 29, the realignment of Wall Street, the roadway connection to US 141, and the redevelopment of the residential area along the future CTH EA and Buddy Lane.

- *Regional Coordination:*
  - Stream crossings: The roadways for Phase III of the light industrial area include two stream and ESA crossings. The Phase III stormwater pond is located near a stream. WDNR coordination and approval are necessary.
  - STH 29: Improvements to STH 29 east of CTH EA bordering Bellevue will be a joint effort of the Town, Village of Bellevue, Brown County, and WisDOT. The roadways should be coordinated with the Phase III light industrial development and the proposed Bellevue commercial park.
  - Rail line crossing: Connecting the southern Phase I light industrial and northern Phase III light industrial areas will require a crossing over the rail tracks. It is unclear whether the rail line will still be in active use by Phase III. The developer and Town will need to get approval from the owners of the railroad (currently Fox Valley and Western) and WisDOT.

#### *As Opportunity Presents*

Some implementation steps are not directly connected with the designated Future Business Park parcels, but are critical to eventual build-out and expansion of the business park. The Town should pursue these opportunities at their first availability:

- It is recommended that the open space at Cottage Road and STH 29 be removed from the Future Business Park designation. However, the acquisition and development of the open space is directly related to likely future expansion of the business park. The open space acquisition should occur in the near term before development occurs.
- The Fox Valley and Western freight rail line crosses the study area. It currently provides service for one user in the Denmark area, but has very low usage. When the rail line is no longer in usage, the Town should coordinate with the county and state to purchase the rail line right-of-way and convert it to a multi-use trail.

#### *Coordination of Private Development Interests*

Although the Town has established the vision and set a bar for the quality of development within the business park, private landowners and developers will drive the timing and success of the business park. While ensuring a consistent quality of development around the Town's only interstate interchange, the Town should collaborate with private landowners and developers. To coordinate and support the private sector's development expertise and creativity, the Town should consider alternate public spending priorities and moderate changes to the master plan. The master plan should be considered a living document that can adapt to the needs of the regional marketplace, the success of the business park, the needs of the land, and the desires of Town residents and its elected officials.

## ***Marketing Plan and Strategy***

At this time, the Town does not own any of the parcels within the business park and does not expect to acquire and develop parcels. Without land control, the Town cannot directly develop areas of the business park. The Town could assume a passive role, waiting for development applications from landowner/developer teams. The Town would play a reactionary role, inserting its vision of the business park midway through the development process.

Should the Town wish to take a more aggressive marketing strategy, this section offers a plan to connect pre-qualified developers with interested landowners. Under this strategy, the Town seeks out potential developers that are likely to understand the Town's intents and development guidelines and introduces them to interested private landowners.

To market the business park, the Town can dedicate staff resources, it can appoint a volunteer committee of interested Town residents, or it can hire a consulting firm to implement a marketing plan. No matter the approach, there should be a primary contact person or firm to distribute materials and receive and respond to inquiries. Lack of a contact person/firm will give the business park implementation an unorganized and unprofessional appearance and may result in inconsistent direction being given to private landowners and prospective developers.

A first step is to develop coordinated marketing materials for the business park. The first piece should be a brochure that will interest quality developers to the sites. Marketing materials should include a brochure showing the map of the development including a description of the planned development for each area including the net acreage sizes, a visual representation of land use plans and development guidelines, and contact information. The brochure could direct readers to the Town website for further graphics and the latest information on the business park.

The Town or its marketing consultants should prepare a comprehensive list of local and regional developers. The list can be compiled from previous successful experience within the Town and in the region, recommendations from other towns, villages, and cities, contacts through economic development and chamber of commerce connections (e.g. Advance, the economic development branch of the Green Bay Chamber of Commerce), and other methods. In order to elicit interest in the business park project from developers, the Town or its marketing consultant should create and send a prospecting letter and brochure to these potential developers. Developers that respond with interest should be sent additional marketing materials.

The Town should develop a second set of coordinated marketing materials for developers that express interest. The marketing materials should include a historical description of the sites and the Town, additional information on potential development opportunities, and the Town's priorities for development. The marketing materials should provide a clear articulation of what improvements are to be provided by the Town and their cost to the developers and what improvements will be the responsibility of the developers. The materials will need to address what additional resources are available to developers and what the expected relationship will be between the each developer, each private landowner, and the Town.

The Town should interview selected interested developers. When assessing potential developers, the Town could consider the following criteria:

- How well the developer understands the Town's goals and guidelines as defined in the master plan
- Financial capacity of the developer
- Track record of the developer for the past five years
- References – testimonials from past clients
- Review of the developer's project most similar to the Ledgeview Business Park

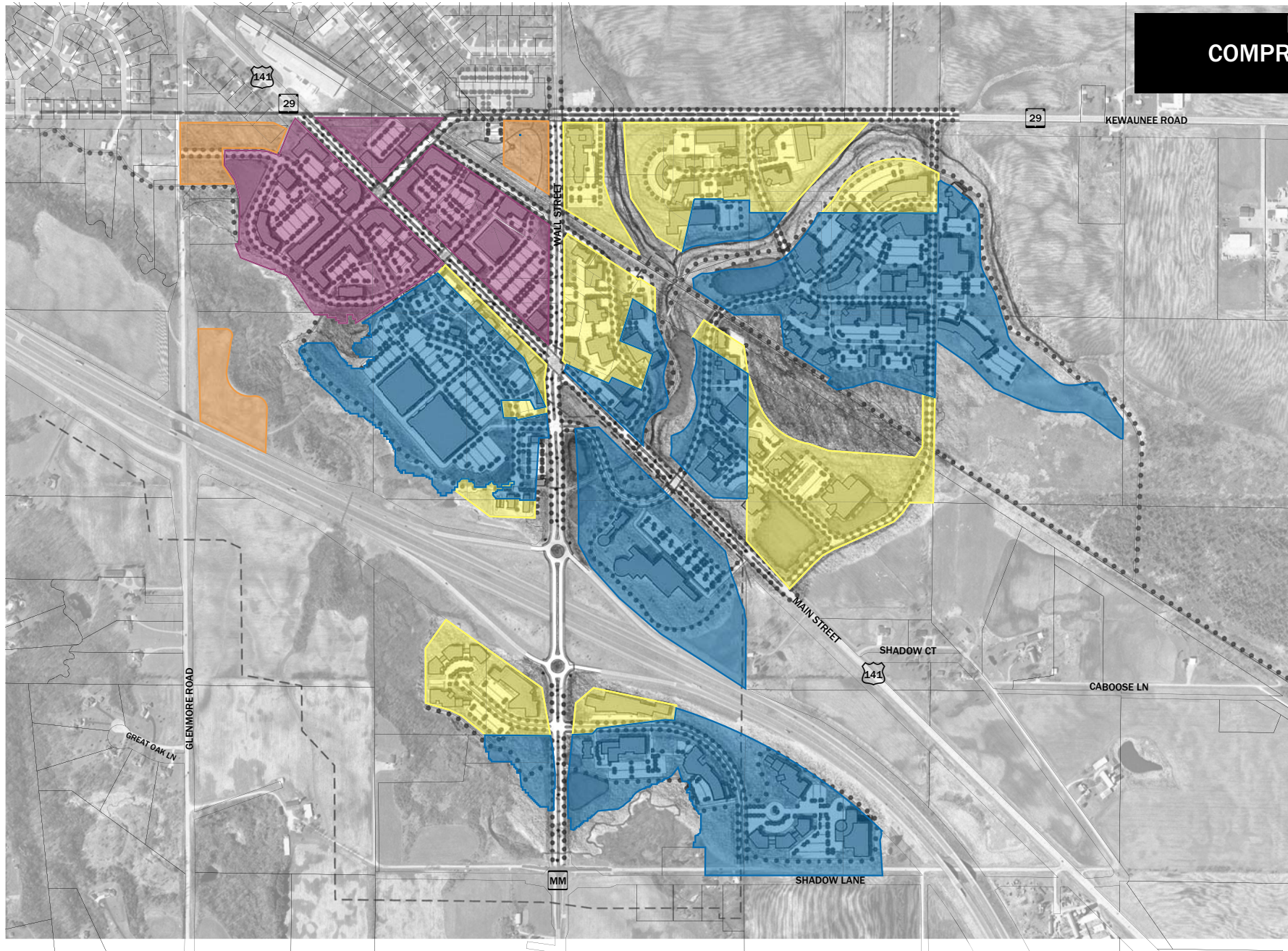


- Experience in areas of proposed specialty – commercial, office, light industrial, mixed use or a combination of all specialties

Pedestrian-oriented and mixed-use development projects are typically more complicated and difficult than auto-oriented and single-use development projects. The Town should seek out developers that have successful experience with mixed use and pedestrian-scale mixed-use development in Phase III. The Town should encourage developers of successful NCD projects in the GV corridor to consider also developing in the Ledgeview Business Park.

For developers that are likely to meet the expectations of the Town in the business park master plan, the Town should introduce them to private landowners. The Town should consider the land use specialty of the developer, the scale of past development projects, and the combined temperaments of the developer and landowner. The developers and private landowners will create their own contracts for planning, entitlement, and development. The Town may offer assistance to landowners in understanding the components of development and the typical roles of developers in the process.

Of course, private landowners are free to contract development services from any developer, or may wish to develop their land themselves. In development review and approval processes, the Town must treat each developer/landowner fairly, regardless of whether the Town prompted the pairing or not. It is expected that review and approval processes will be smoother and quicker for pre-qualified developers that have experience in creating quality commercial and mixed-use areas.



# RECOMMENDED COMPREHENSIVE PLAN AMENDMENTS

## LEDGEVIEW BUSINESS PARK

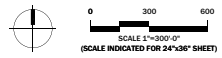
Town of Ledgewiew  
Brown County, WI

Project # 2134  
June 19, 2006



### Legend

- Maintain Designation
- Remove Future Business Park FLU Designation
- Add Future Business Park FLU Designation
- Add Future Neighborhood Center FLU Designation



**Figure 5**





# PHASING PLAN

## LEDGEVIEW BUSINESS PARK

Town of Ledgeview  
Brown County, WI

Project # 2134  
June 19, 2006



### Legend

Currently Designated  
Future Business Park

- Phase I
- Phase II
- Phase III

Recommended Future  
Business Park Expansion

- Phase I
- Phase II
- Phase III

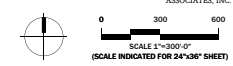
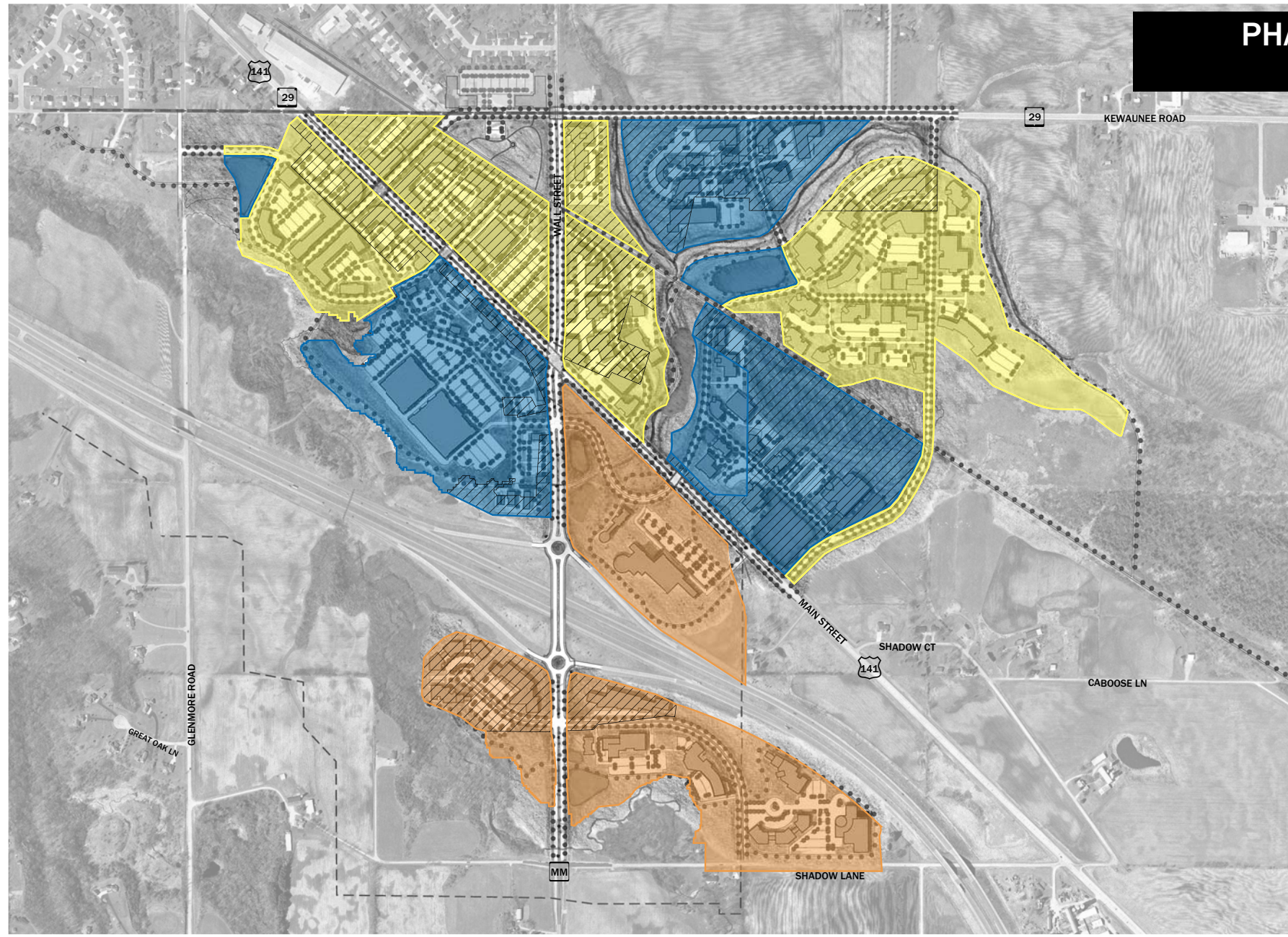


Figure 6



## **APPENDIX**

Ledgeview Business Park Development Guidelines

Town of Ledgeview Neighborhood Center District Concepts for Development (February 21, 2006)

Detailed Cost Estimates

Town of Ledgeview Resolution







# Ledgeview Business Park Development Guidelines

**July 2006**

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## Introduction

The Ledgeview Business Park Development Guidelines govern development and redevelopment activities on parcels within the Ledgeview Business Park. The boundaries of the business park are shown on the Future Land Use map of the Town of Ledgeview Comprehensive Plan. See the Ledgeview Business Park Master Plan for complete implementation information.

These guidelines are stricter standards for building and design, landscaping, and urban design. These guidelines have been adopted through incorporation into the implementing Planned Development District Business Park(PDD-BP).

There are three sections to these Development Guidelines.

- **Character Images:** These images depict the intended character of all areas of the business park. Future development plans should include the materials, open space design, interaction with the natural environment, and general character shown in these images.
- **Design Guidelines:** The images and text describe the required design components of development within the business park. The design guidelines speak to site design, building design, access and parking, streetscape and open space, and signage and lighting. The section begins with general design guidelines that are applicable throughout, followed by characteristic design guidelines for each district.
- **Streetscapes:** The final section describes the intended streetscapes for the existing regional roadways and new streets within business park. The guidelines include designs between the street curbs and between the curbs and the edge of the right-of-way.

## Purpose

The Ledgeview Business Park Development Guidelines serve to guide land owners and prospective developers of property within the Ledgeview Business Park. These guidelines are designed to create a better quality of design, from the site to the regional level. The development guidelines aim to safeguard property values, protect public and private investments, and promote high quality commercial, residential and industrial development consistent with the desired character of Ledgeview.

## Applicability

The development guidelines aim to further land use and development controls in the Ledgeview Business Park. These standards are in addition to already adopted commercial and industrial standards in the Ledgeview Code and the design standards of the Neighborhood Commercial District or Planned Development District (as applicable). All properties within the Business Park will be subject to a design review process based on criteria specified in the zoning code.

The design review process shall also be required when existing commercial and industrial uses undergo additions and alterations to roofs and exterior walls, which had previously not been subject to design review.

The development guidelines shall be adhered to before a building permit is issued to new commercial, industrial or residential development within the Business Park. Also, no site work shall be allowed such as tree removal, grading, or excavation until design approval is received.

## Ledgeview Business Park





Office buildings should have variations in building massing and formal landscape with pedestrian friendly entrances.



Gas station with an interesting roof form.



Water fountains should be used as entrance landscape feature.



Natural elements such as stone and water should be used as part of formal landscape.



The use of natural materials is encouraged. Various combinations of stone, wood, water, native plants and trees shall be used to create formal and informal landscapes. The image on the left is an example of a formal landscape in a corporate campus.



Building materials such as brick, stone, exposed concrete and glass are suitable for corporate buildings. Massing is as important as the materials to create an attractive facade. Changes in building heights and setbacks help develop an attractive massing.



Natural elements should be used to create entrance signs to various districts in the business park (left).





Entrance signs using natural elements and materials help frame the entrance to a big-box retail center.



Colored pavement and landscape begins to generate a pedestrian friendly environment. Street furniture and planting beds can be used in areas such as the village center and corporate campus.



Use of natural stone to create entrance signs like this is encouraged.



The theme depicted in the Business Park stresses on natural materials, forms and textures, and colors.

From entrance signs (left) to landscape plazas (bottom left) and building facades, use of locally available materials and organic forms is encouraged.



This would help to define and reinforce the character of Ledgeview.



Big-box retail stores should develop natural landscape at entrances and also within the parking areas.





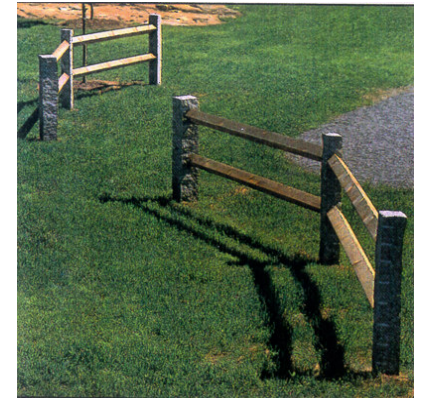
Pedestrian trails along the ravine edge help maintain a natural aesthetic. Interpretive signs describing the geology, biology or ecology of the area are helpful.



Stone and water as landscape elements in open space areas.



Formal landscape with pedestrian paths in the corporate campus space. Detention ponds become part of the landscape.



Stone fencing used to loosely define trail edges.



Pedestrian and bike trails should be encouraged in the Business Park. They shall run along the natural edge wherever possible to encourage pedestrian activity. The natural edge is the land along the streams and the ravines.



Parks and open spaces with landscape features such as water fountains and play areas for kids are encouraged. Again, the predominant materials used for landscaping are stone and native plants. Different types of open space exist in the Business Park, depending on the location and land use.



Detention ponds used to store water runoff can be integrated into the landscape either to create attractive entrances or water features.





Gateways mark the entrance to various areas. This design uses local materials and is derived from local forms.



Corporate landscape uses same elements but is more formal in character.



Variations in height and material should be encouraged. Natural materials for facades is also encouraged.



Structures shall be part of the landscape using materials such as stone and timber.



Big box retail parking areas should incorporate landscape in parking lots. This could be in the form of trees, plants or grass.



Informal pedestrian connections between different land uses can be landscaped using stone, trees and native plants.



The retail development (above) provides a good example of building massing, use of materials and landscaping. Setbacks, roof forms, materials, paving, and street trees create a high quality design.





The Village Center would consist of mainly two to three storeyed buildings with minimum setbacks, and a pedestrian friendly environment.



Water fountains and plantings help create an attractive Village Center.



Brick paving, street furniture and crosswalks help create a pedestrian friendly setting.



Textured pavement combined with rough cut stone sculptures can be used in corporate campuses.



Attractive buildings frame the main street, having a greater level of architectural and landscape detail

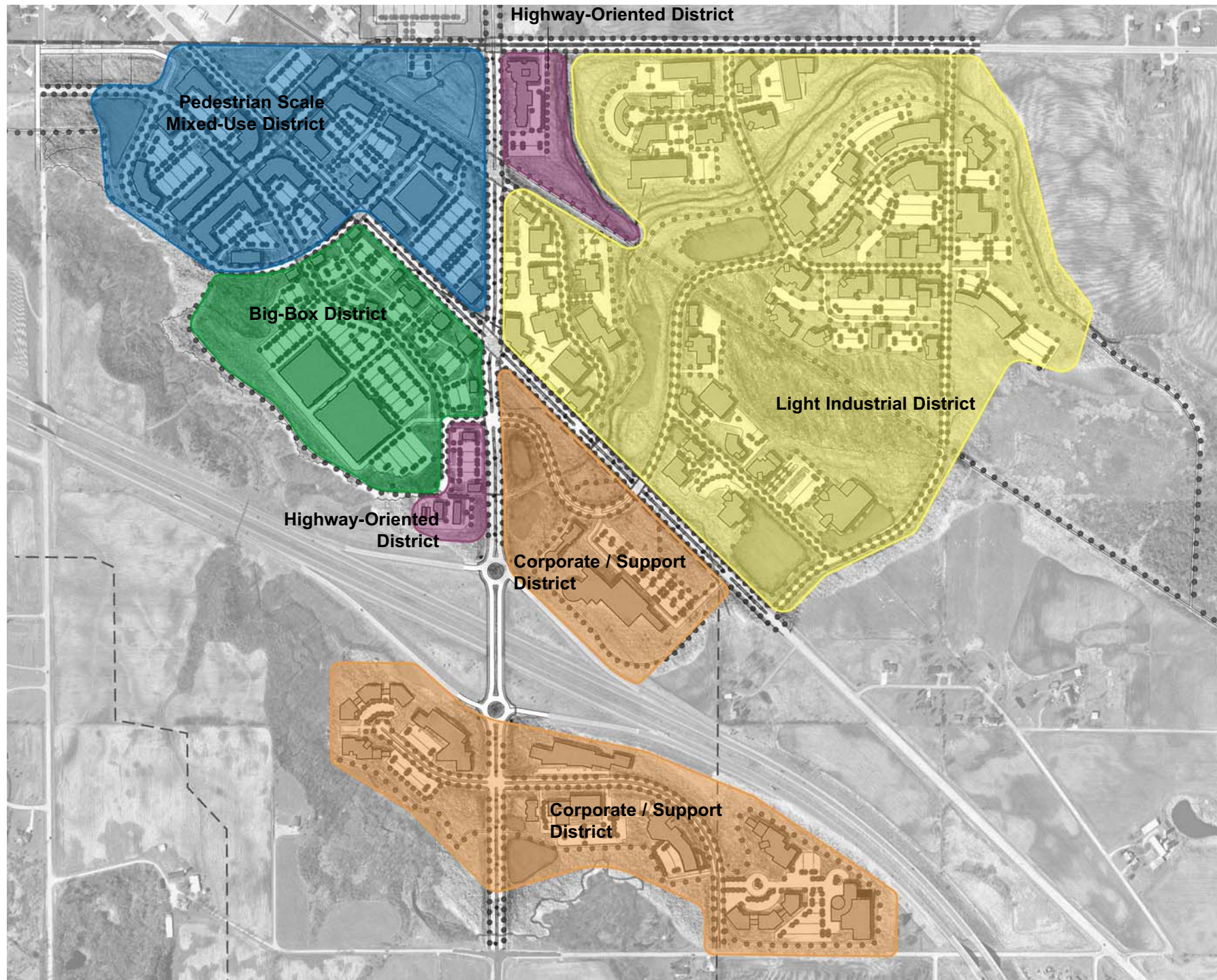


Roundabouts can be used as traffic calming devices and to create a focal point, either in the form of a fountain or a sculpture.



The central plaza incorporates elements such as lighting, sculptures, water fountains and planting beds. These elements combine to create an attractive congregation space in the Village Center.





## General Design Guidelines

(Applicable to development activities in all districts of the Business Park)

### Site Design

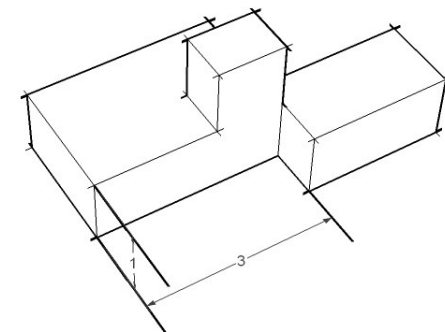
- Wherever possible, buildings should be parallel to the street.
- Locate service areas and refuse containers at the rear end of the site and screen from public view.
- Encourage storm water management treatment systems that focus on Best Management Practices (BMPs), e.g. natural landscaping to increase infiltration and reduce run-off.
- Landscape buffers and screens should be diverse in terms of elements. This shall include a combination of trees, shrubs, fences, ornamental masonry, landscape berms.
- Where appropriate, the site design should address or incorporate existing natural features such as forested areas, streams and topography.

### Building Design

- All architectural elevations of buildings should consist of a base, body and cap. The base and the cap should be clearly distinguishable from the body through changes in color, material, profile or texture.
- Changes in massing should be related to entrances, the integral structure and/or the organization/architecture of interior spaces and not merely for cosmetic effect.
- Horizontal massing shall not exceed a height: width ratio of 1:3 without substantial variation in massing that includes a change in the height and building setback.
- Screen ground mounted or wall mounted equipment and architecturally integrate into building design.
- The use of low reflectance material, subtle, neutral, or earth tone colors as the predominant colors on the façade is encouraged.

### Access and Parking

- Provide landscaped buffers between all public streets and building/structure or parking.
- Landscape edges of parking areas and within each parking island.
- Pedestrian access should be provided both within the site and between adjacent sites.
- Sidewalks are required on front/main entry of all buildings.
- Coordinated pedestrian circulation should also be promoted between adjacent lots.



Height:width ratio of 1:3 should not be exceeded. This can be achieved as shown above by height variations and setbacks.



## General Design Guidelines

(Applicable to development activities in all districts of the Business Park)

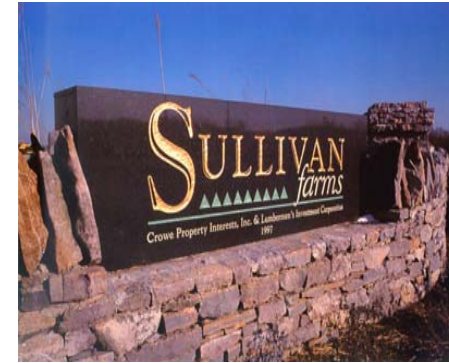
- Landscape berms, walls, fences and other forms in parking areas should be designed to allow pedestrian passage.
- Plans for parking areas should include a description of snow removal methods and storage areas such that they don't harm or kill plants.

### Streetscape and Open Space

- Landscaped elements shall be replaced and maintained in a timely manner.
- All public streets should incorporate streetscape elements such as trees, street lighting, plantings, and signage.
- The property owner or lessee of the building shall be jointly responsible for maintenance of the parking area, accessways, striping, landscaping, screening and required fences.
- Convey storm water to on-site infiltration areas which should be designed as site amenities.

### Signage and Lighting

- All signage should complement the theme of the business park.
- Integrate signage location, form, and material with the building design.
- Design and position signs such that they frame the front edge of the site. The same treatment should be done using street lighting such that it defines the street edge.
- Billboards and other off-site signage are prohibited.
- No sign shall be located as to restrict sight lines and orderly operation and traffic movement.
- All exterior lighting should balance on-site needs for safety, security and aesthetic effects.
- Exterior architectural lighting for buildings and landscape shall be ground-mounted. They shall be indirect, focussed in a manner that the source of light is not visible from the street or adjoining property.
- Street and sidewalk lighting should be of a complementary style and color scheme, depicting the theme of the business park.



The material, texture and color of signage should be consistent with the theme of the business park. This means more earth tone colors, rough texture and stone (mainly) signages. The examples above show different ways to achieve this.



**Highway-  
Oriented  
District**



## Highway-Oriented District

### Purpose

The design guidelines for the Highway-Oriented District aim to protect and enhance the character and quality of commercial districts where gas stations, convenience stores and outlots are located.

### Site Design

- Buildings should front the public streets such as CTH MM, I-43 and USH 141 and be buffered by landscaping.
- Wherever possible, locate the gas pumps behind the store or to the side and bring the store close to the public street (CTH MM or I-43).

### Building Design

- Building facades facing major roads such as CTH MM and I-43 should receive full design consideration. This means that the facades should be subdivided and proportioned using features such as windows, frames, sills and lintels, shading devices, and modulations of the walls.
- Create a design character easily utilized for a wide variety of businesses. Avoid franchise design that signifies a particular brand or product.
- Storefronts should be designed with a theme similar to the rest of the Business Park in terms of use of materials, architectural elements, building massing, and landscaping in and around the site.
- The design of pump islands should be architecturally integrated with other structures on-site using similar colors, materials and details.
- Drive through elements should be architecturally integrated into the building rather than appearing to be applied or "stuck on" to the building.



Landscape buffer between building and main street screens parking areas.



Use of materials and design elements that avoid typical franchise character



Integration of pump islands with other structures using colors, materials and details

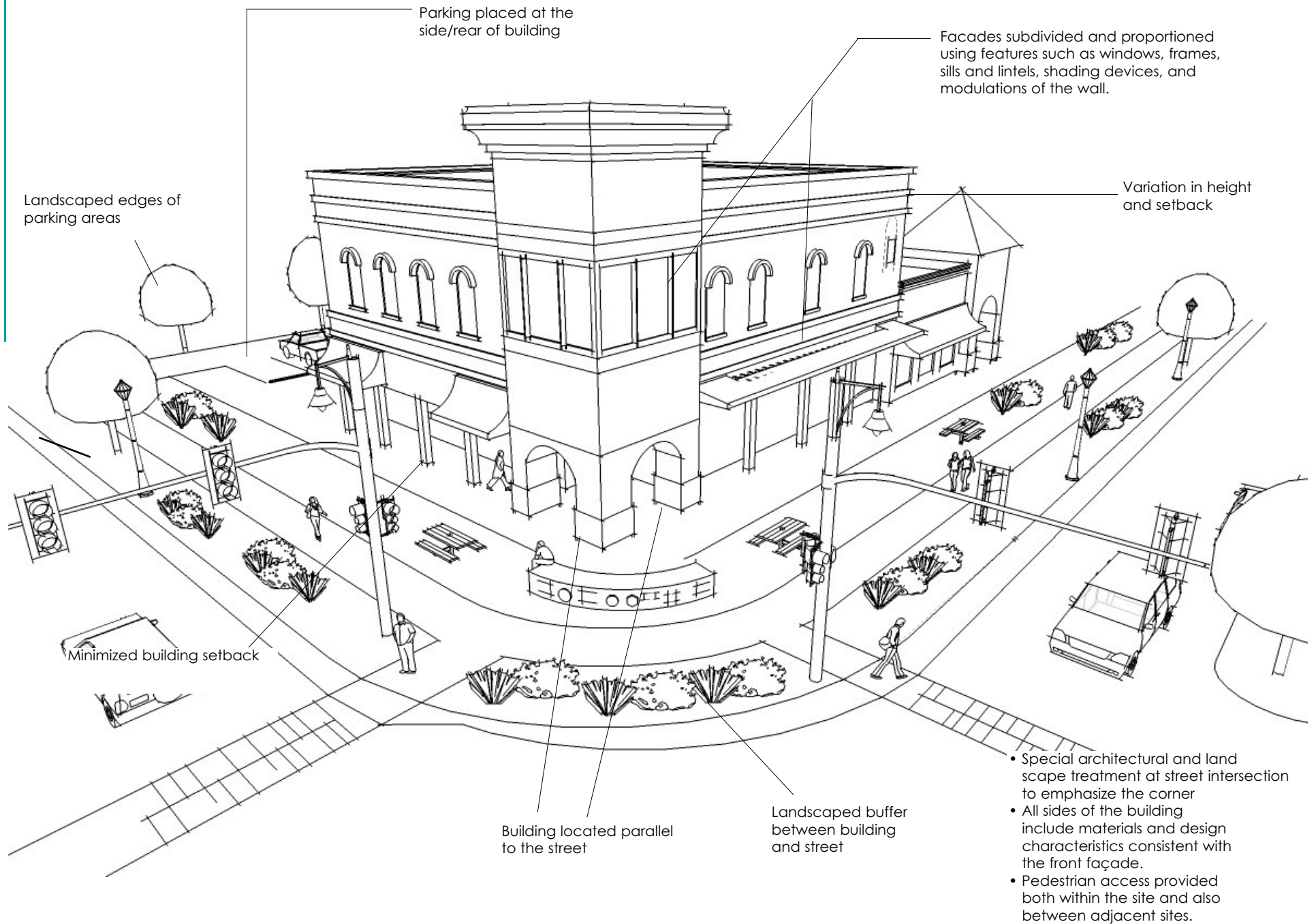


## Highway-Oriented District

- [illegible]

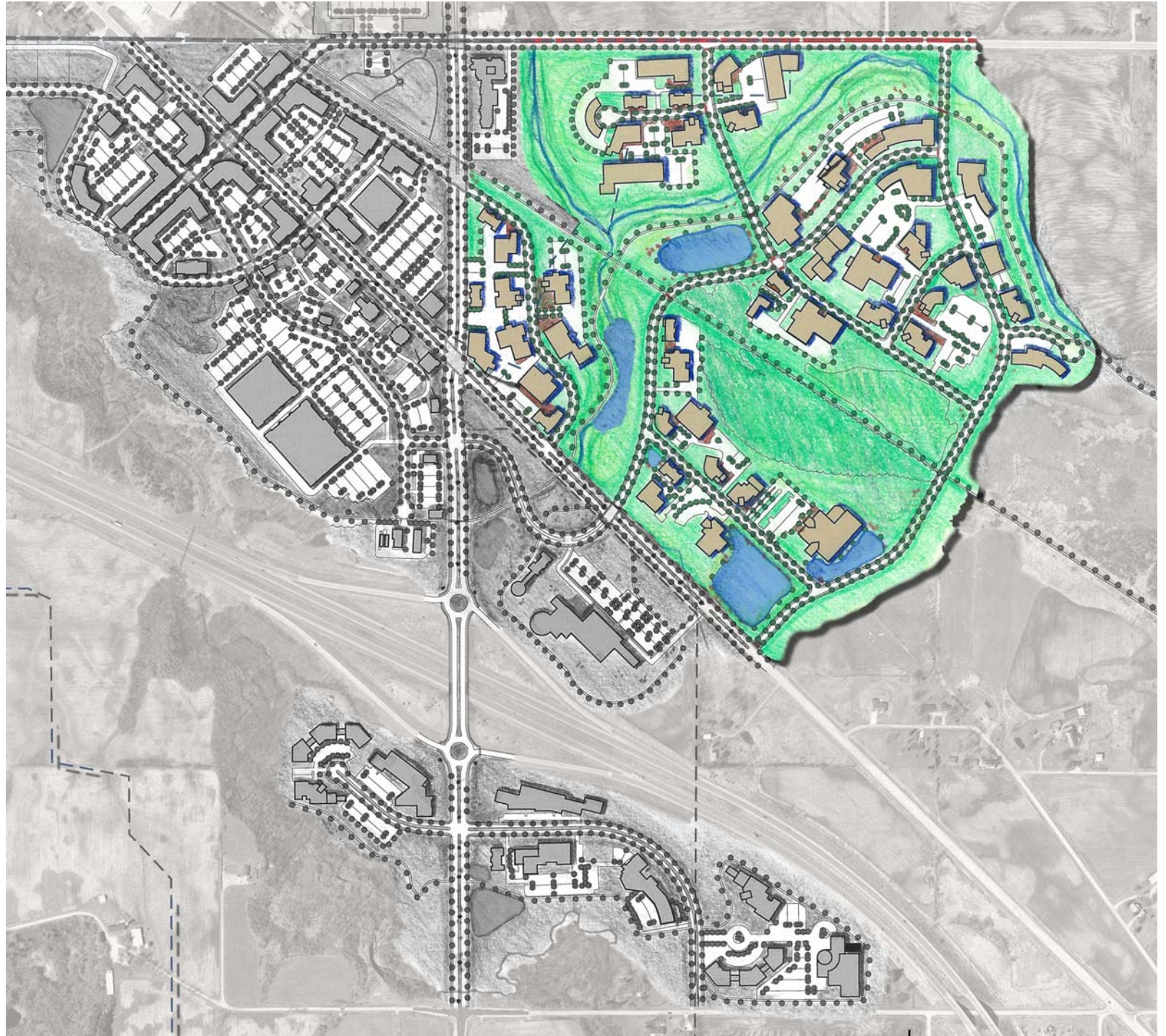
12







**Light  
Industrial  
District**





## Light Industrial District

### Purpose

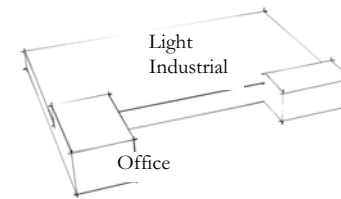
The design standards for the Light Industrial District promote a consistent and appealing site and building design while encouraging economic development.

### Building Design

- Building facades facing STH 23, USH 141 and CTH MM should receive full design consideration. These sides should have detailed facades, good quality materials and construction, and incorporate landscape elements.
- Larger industrial buildings shall avoid monolithic appearance on frontages and rooflines. The larger "box" type massing of such structures must be offset by breaking up building sections, or by the use of elements such as variable planes, projections, setbacks, and changes in roof lines.
- Encourage visually interesting roofs, provide variations in the roof line and incorporate treatments such extended eaves and parapet walls with cornice treatments.
- The clustering of smaller, visually compatible structures is desired over singular large structures.
- Larger industrial buildings should have a small office component fronting CTH MM and USH 141 . This office façade shall be subdivided and proportioned using architectural features such as windows, entrance features, arcades, porches, or treillage with vines along no less than 50% of the façade.
- No single establishment shall exceed a building footprint of 150,000 square feet as defined by the exterior walls. The office component shall not count towards the 150,000 square feet.

### Access and Parking

- Plant materials used for screening must be of a suitable size and density to accomplish screening within five growing seasons.
- Locate parking areas at rear of building wherever possible. Where necessary, locate parking at the side of building and provide a landscape buffer between the parking and any public street. Offstreet parking in front of buildings shall only be considered when no other design option is possible.



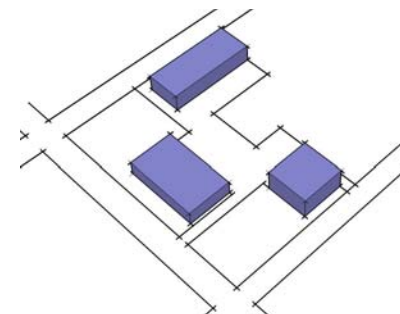
Small office component fronting public street with industrial uses behind



Office component with higher level of architectural detail and design



Screening mechanical equipment and refuse containers



Adjacent parking lots linked to promote internal circulation and shared parking

## Light Industrial District

- Adjacent parking lots should be linked to provide internal traffic circulation.
- Connect building entrances to sidewalks and cross walks.
- Within the site, pedestrian access should be provided from the parking lots to building and between the different parts of a building complex.
- Off-street parking spaces shall not be used for open storage, a sale or rental of goods, or storage of inoperable vehicles without approval of the site review/zoning and Planning Commission.

### Streetscape and Open Space

- Create pedestrian/bike trails through open space areas and along the wetland edge, allowing for future connections to other areas.
- Plants which provide interest in structure, texture and color should be planted along all public streets and in larger open spaces.
- Where appropriate, site design should integrate the proposed landscaping/open space with the adjacent landscaping, open space or natural feature (wetland, forested area)
- A minimum of 25% of a site should be dedicated to open space (areas excluding buildings, roads and parking areas). 40% is considered desirable.
- Landscape private streets with street trees.
- In cases where large buildings are set back, add dense landscaping such that they define the street edge.

### Signage and Lighting

- Signage should indicate only the name of the business, nature of business and/or the street address.
- All exterior lighting should be a part of the architectural and landscape design concept in color, location and type of lighting.
- Promote lighting fixtures which complement the street lights in terms of materials, form, color.



Landscaped parking edge



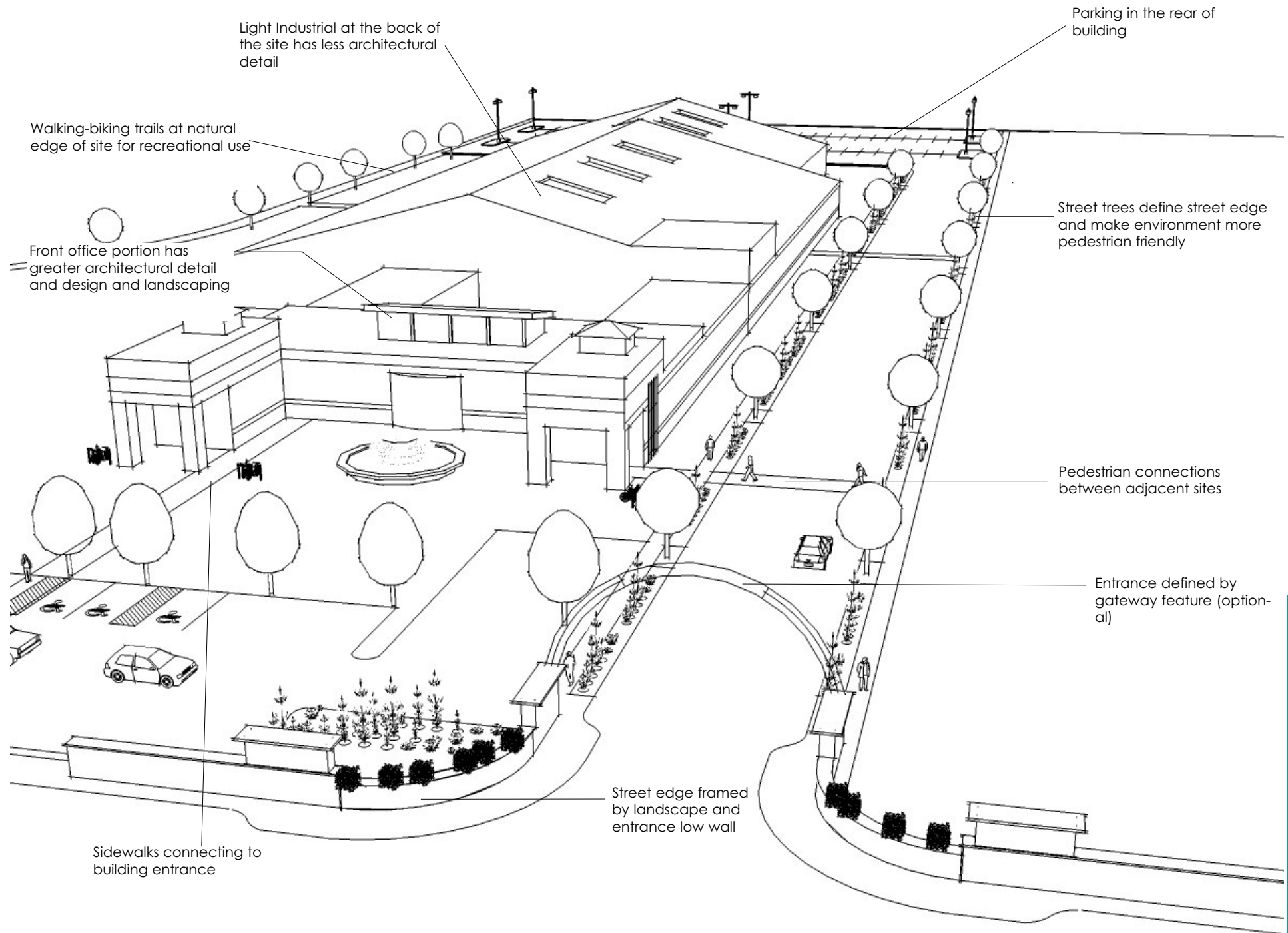
Pedestrian trails along ravine edge



Incorporating storm water ponds into landscape



Use of color, material and landscape should be similar to the buildings in the District





**Corporate/  
Support  
District**





## Corporate/ Support District

### Purpose

This district is part of the entrance corridor into the Ledgeview Business Park and Green Bay region. Large corporate campuses with high quality design along I-43 should aim to create an attractive and desirable gateway.

### Building Design

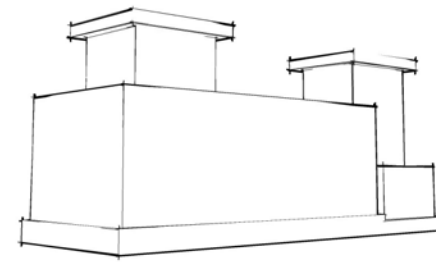
- High quality architecture should be promoted as the buildings are within a gateway viewshed into the Town of Ledgeview. Quality in terms of style, construction, materials and landscape.
- The use of high quality building materials such as brick and native stone is encouraged.
- Buildings occupying more than 25,000 square feet and/or 60 feet or more of street frontage should be designed with recess and projections, material changes, and other articulations every 30 to 60 feet. This helps break up large masses and creates the appearance of smaller buildings.
- In order to promote human scale, large blank facades need elements that provide visual interest. Human scale and visual interest can be achieved if the façade is subdivided and proportioned using features such as windows, frames, sills and lintels, shading devices, and modulations of the wall.
- Encourage visually interesting roofs, provide variations in the roof line and incorporate treatments such as extended eaves and parapet walls with cornice treatments.

### Access and Parking

- Provide a landscaped buffer between I-43 and CTH MM and building/structure or parking.
- Locate parking areas at rear of building wherever possible. Where necessary, locate parking at the side of buildings and provide a buffer between the parking and public street.
- Adjacent parking lots should be linked to provide internal traffic circulation.



High quality architecture in terms of design, materials and construction quality.



Variations in roofline and building massing helps create a visual interest.



Above and Below: reducing run-off in parking lots using landscaped islands



Town of Ledgeview

## Corporate/ Support District

### Streetscape and Open Space

- The site should incorporate elements such as trees, lighting, plantings, and signs.
- Trees should be planted at a maximum distance of fifty feet from one another along the interior roads (same as PDD).
- Pedestrian trails should be created along the ravine edge for private recreational purposes.
- A minimum of 40% of a site should be dedicated to open space (areas excluding buildings, roads and parking areas).

### Signage and Lighting

- All ground signs should be monument signs.
- All exterior lighting must be a part of the architectural and landscape design concept in color, location and type of lighting.



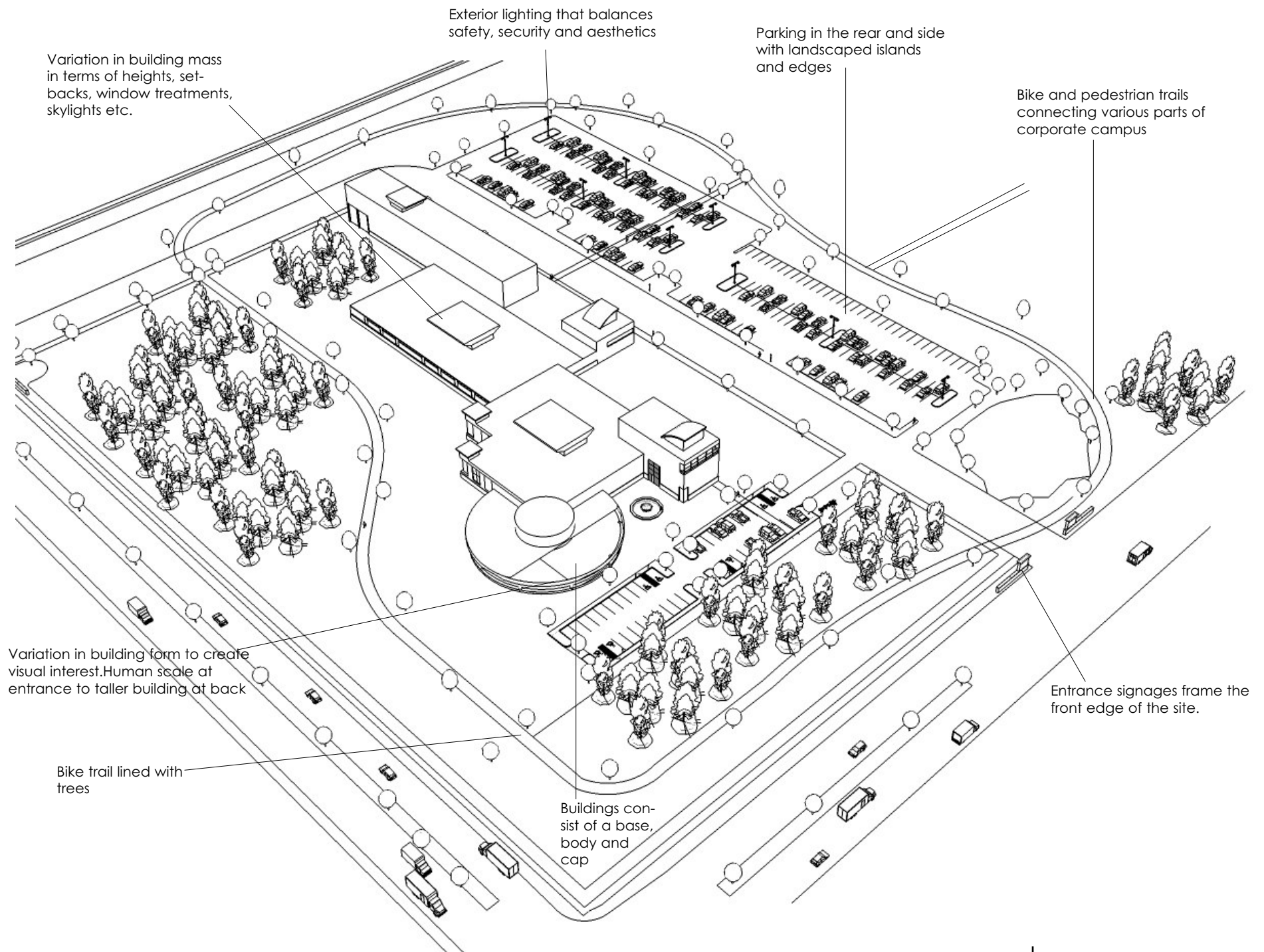
Landscape integrated with detention ponds on site



Above and Below: Entrance signs based on themes









***Pedestrian  
Scale  
Mixed-Use  
District***





## Purpose

The principal purpose of this district is to provide for a variety of pedestrian-oriented retail, lodging, residential and civic land uses in the town center area.  
(from Ledgeview Code)



## Pedestrian Scale Mixed-Use District

Please refer to the 'Code of the Town of Ledgeview' 135-189 - 135-196 for adopted standards and the Town of Ledgeview 'Neighborhood Center District Concepts for Development' (Feb. 21,2006) within the Master Plan Appendix.



Town of Ledgeview

**Big-Box  
District**





## Big-Box District

### Purpose

The Big Box District allows large scale retail formats, but requires a higher design quality and sensibility consistent with the Business Park.

### Site Design

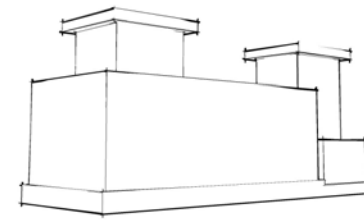
- Encourage storm water management treatment systems that focus on Best Management Practices (BMPs), e.g. natural landscaping of parking areas to reduce run-off..
- Detention basins should be incorporated into the site design in the form a landscape element.

### Building Design

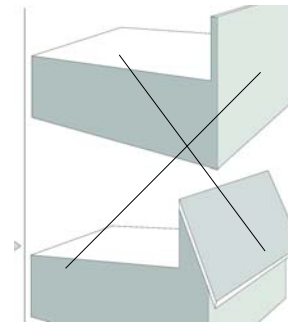
- In order to promote human scale, large blank facades need elements that provide visual interest.
- Human scale and visual interest can be achieved if the façade is subdivided and proportioned using features such as windows, frames, sills and lintels, shading devices, and modulations of the wall.
- The clustering of smaller, visually compatible commercial structures is desired over singular large structures fronting the public street.
- Each large retail store shall have clearly defined, highly visible customer entrances featuring architectural elements such as canopies or porticos, overhangs, arcades, raised parapets, arches or roof forms.
- Discourage fake parapets and facades and create architectural elements which serve a utilitarian purpose, not merely cosmetic.
- Screen ground-mounted or wall-mounted equipment.

### Access and Parking

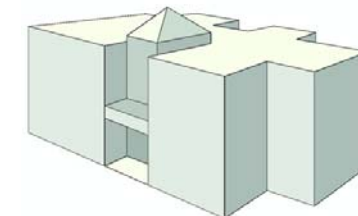
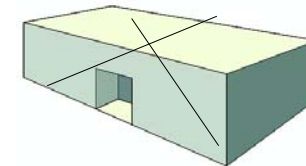
- Provide a landscaped buffer between all public streets and building/structure or parking.
- Allow parking in the front of the store, but break up the parking lot with landscaped islands, trees and plantings.
- Landscape edges of parking areas at in-between islands.



Variations in building height helps to create an attractive building form.



Do not use fake parapets and facades



Avoid large dominant masses by creating variations in setbacks, roof forms and heights.

## Big-Box District

- Pedestrian access should be provided both within the site and also between different sites. Within the site, pedestrian access should be provided from the parking lot to the store. Coordinated pedestrian circulation should also be promoted between adjacent lots.
- Adjacent parking lots should be linked to provide internal traffic circulation.
- Off-street parking spaces shall not be used for open storage, sale or rental of goods, or storage of inoperable vehicles without approval of the site review/zoning and Planning Commission.

### Streetscape and Open Space

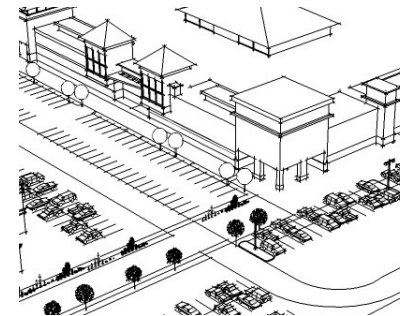
- The site should incorporate elements such as trees, lighting, plantings, and signs.
- Trees must be planted at an approximate minimum distance of fifty feet from one another along the interior roads.

### Signage and Lighting

- All exterior lighting must be a part of the architectural and landscape design concept in color, location and type of lighting.
- Lighting should be designed in a manner which does not permit an adverse effect upon neighborhood properties, especially residential property.
- All ground signs should be monument signs.
- Lighting on site should be used judiciously as needed for safety and information.



Parking lots interspersed with landscaped islands create a better visual character, pedestrian friendly environment and reduce storm water run-off.



Landscape parking areas to break up large lots



Incorporate elements such as signs, public sidewalks, pedestrian access and landscaped buffers between road and building

Variation in massing, window sizes and placement, materials, rooflines help create a better facade

Setbacks in building massing

Architectural design using setbacks, elements such as windows, skylights, roof forms

Parking concealed in the rear

Landscaping to buffer buildings from main street and also to create a pedestrian-friendly environment

Public Road

Pedestrian connections within and to site

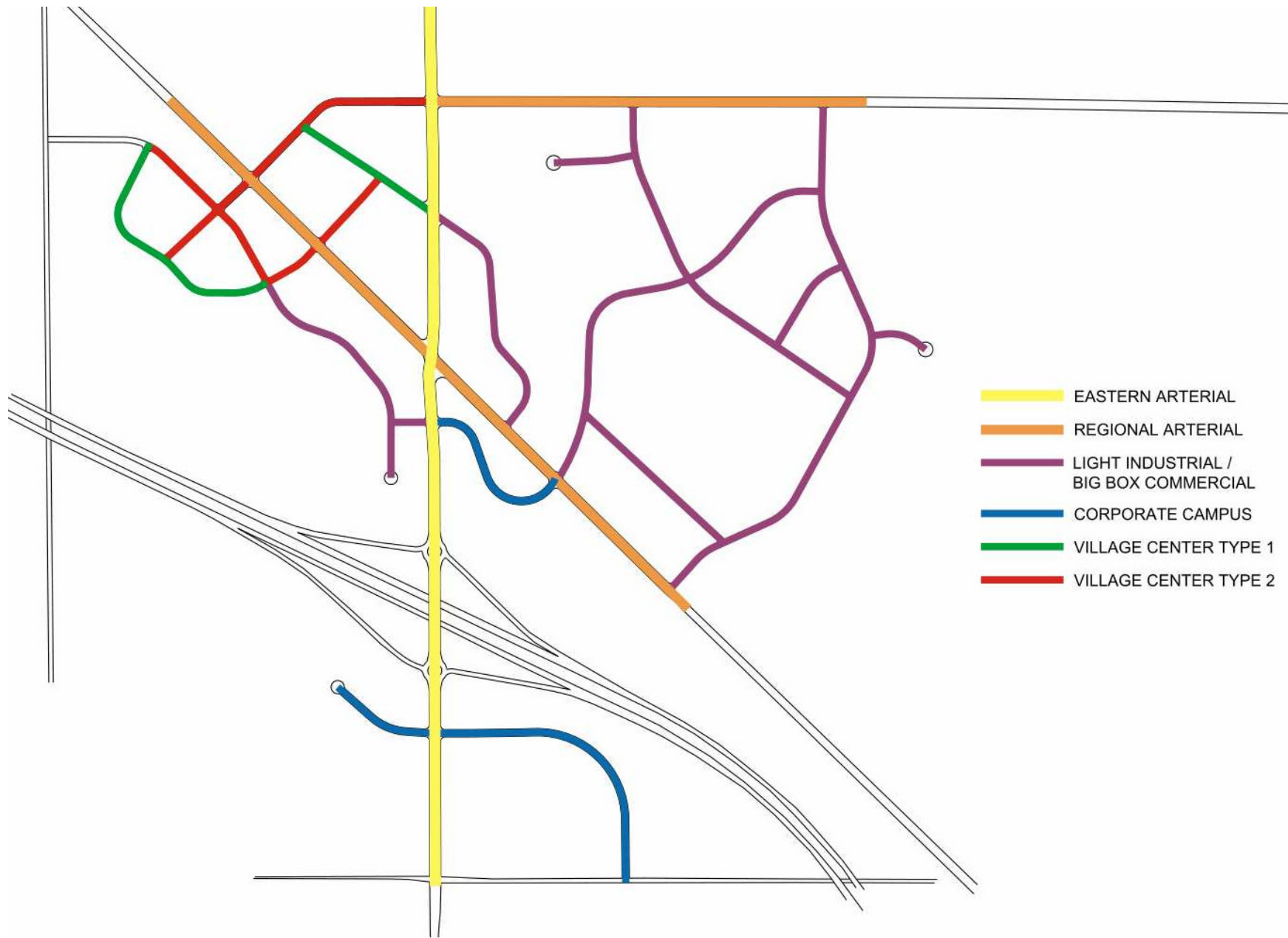
Entrance signages frame the front edge of the site.

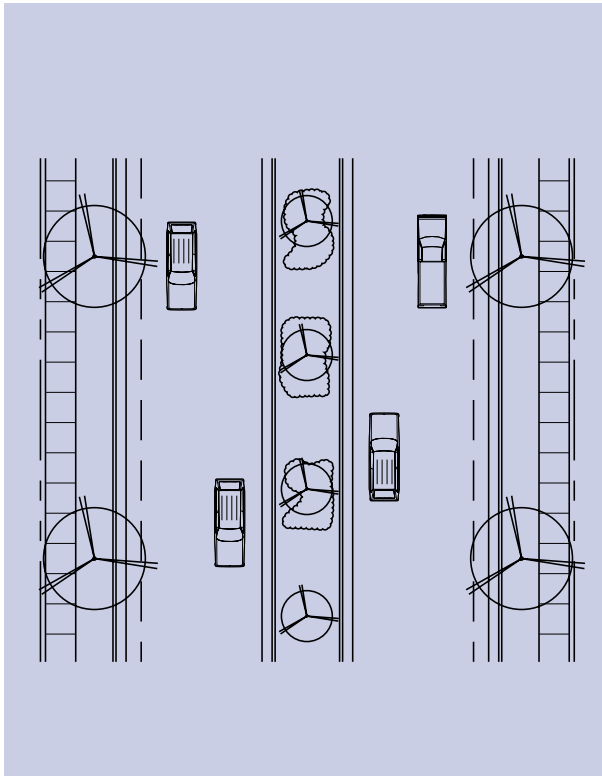
Outlots front main street and partially conceal parking and big box behind





# STREETSCAPE CLASSIFICATIONS



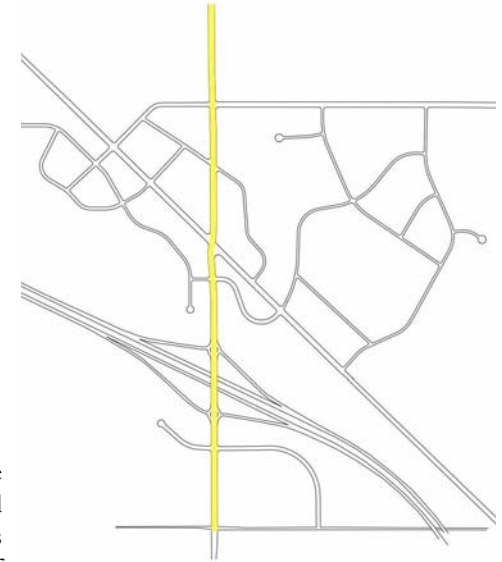


## Design Characteristics:

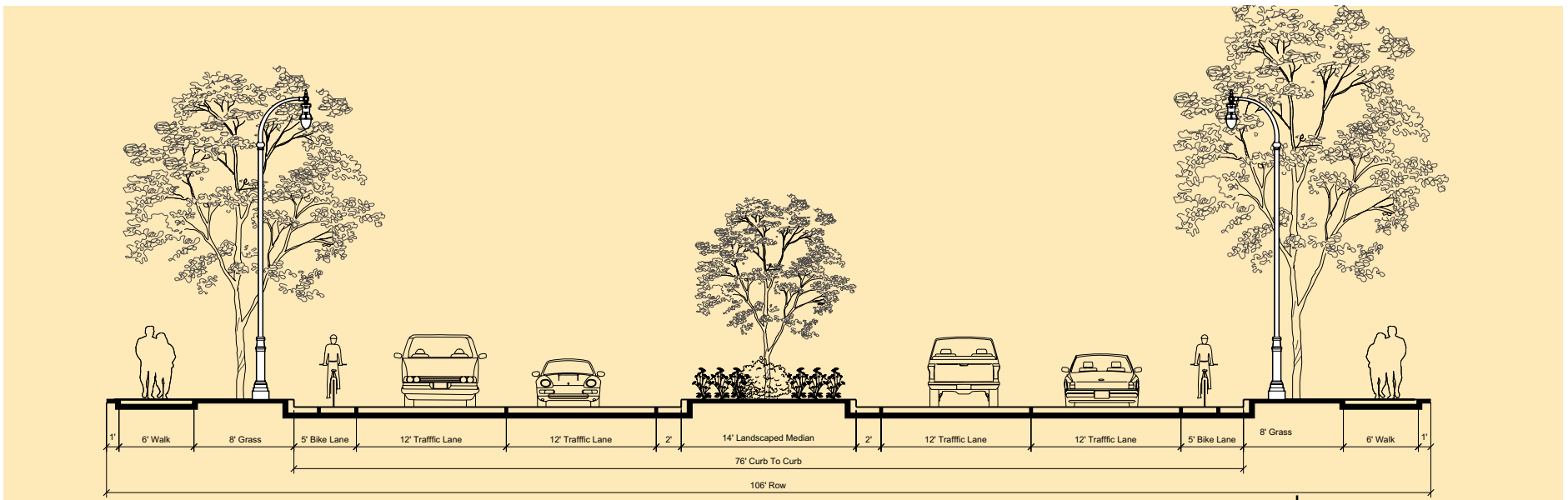
Right of Way	:	106'
Posted Speed	:	35 mph
Street Width	:	76'
Sidewalk	:	6'
Terrace Width	:	8'
Parking	:	no
On-Street Bike Lane	:	yes
Off-Street Bike Path	:	no
Street Trees	:	60' o.c.
Curb Radii	:	25'

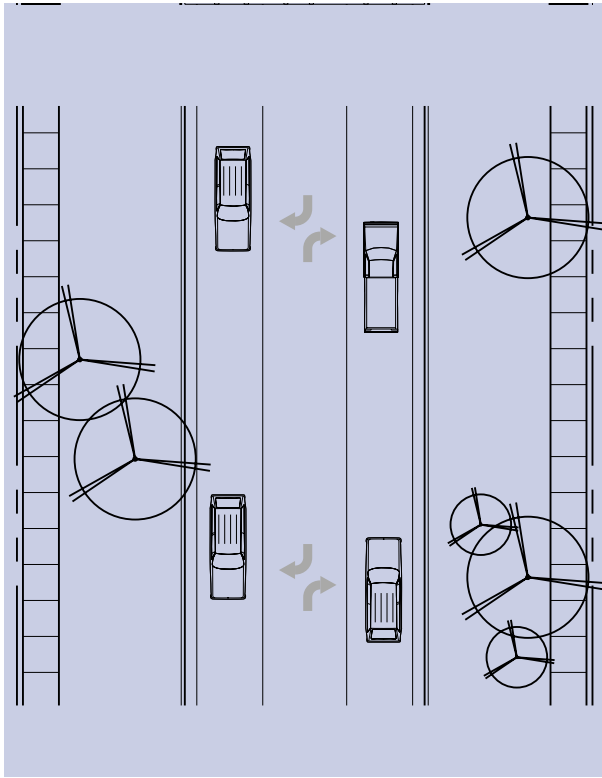
## Description

The Eastern Arterial is the main North-South connection through the Business Park. The proposed uses fronting this street will have limited access points along the street frontage. However, buildings along this street will be designed to parallel to the street and have a higher level of site and building design. Various bike trails running east-west connect to the on-street bike lane.



Location Map



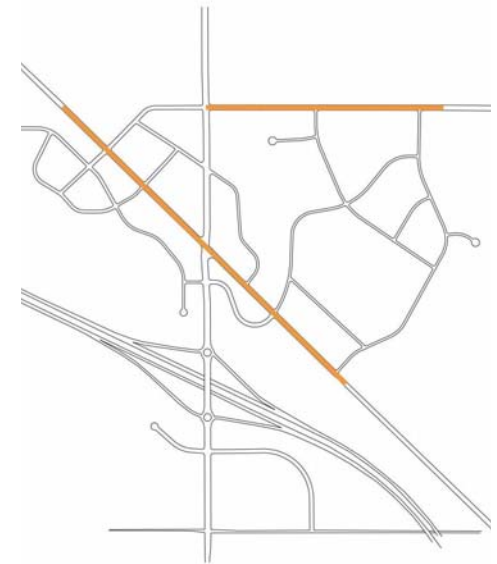


## Design Characteristics:

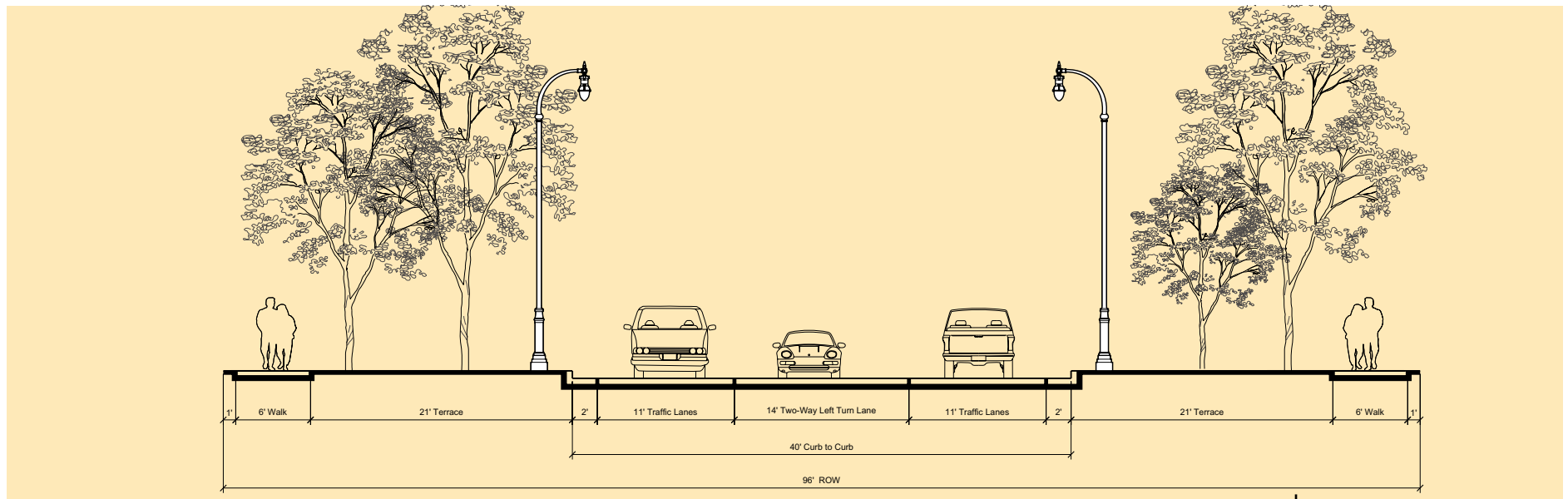
Right of Way	:	96'
Posted Speed	:	35 mph
Street Width	:	40'
Sidewalk	:	6'
Terrace Width	:	21'
Parking	:	no
On-Street Bike Lane	:	no
Off-Street Bike Path	:	no
Street Trees	:	naturalized pattern
Curb Radii	:	25'

## Description

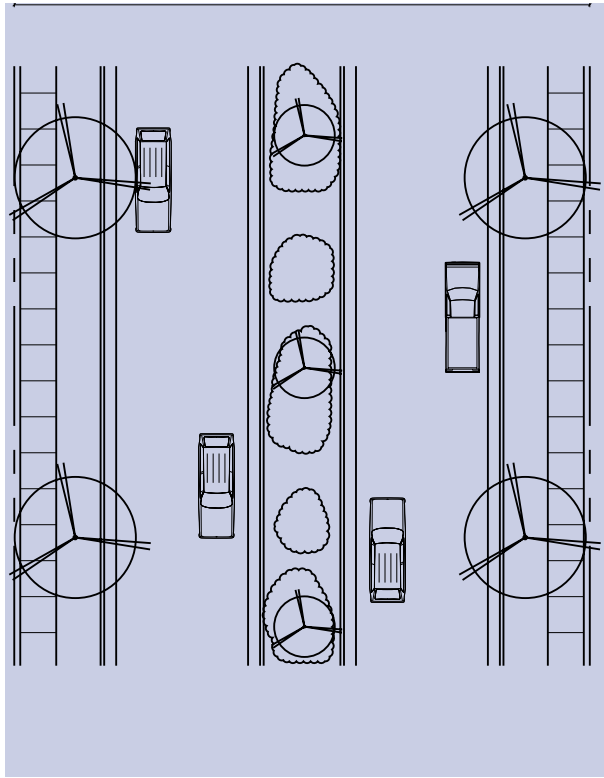
Routes 29 and 141 are currently two-lane rural roads. As the Business Park and the region develops, increasing traffic volumes will likely require a four-lane divided highway. In the interim, this cross-section will direct redevelopment adjacent to Route 29 and 141, while preserving future right-of-way.



Location Map





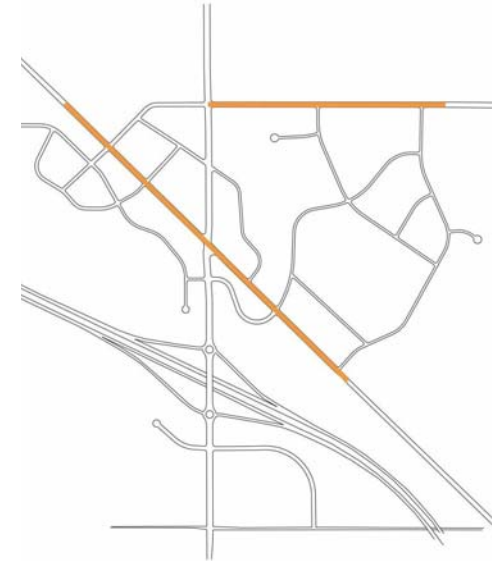


## Design Characteristics:

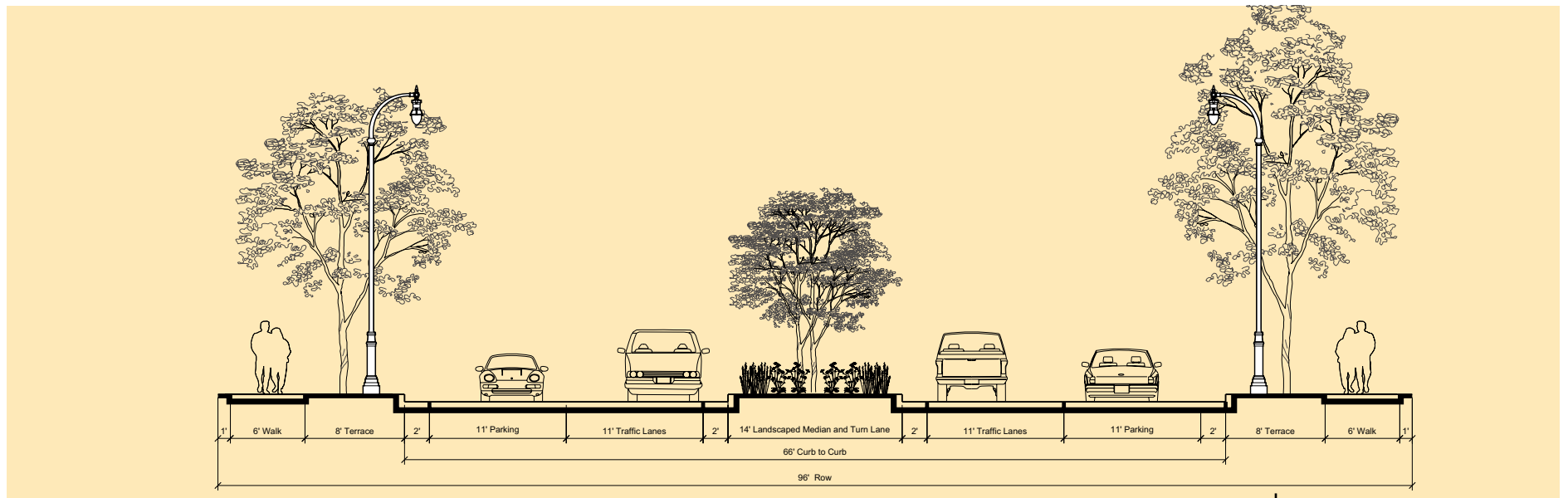
Right of Way	:	96'
Posted Speed	:	35 mph
Street Width	:	66'
Sidewalk	:	6'
Terrace Width	:	8'
Parking	:	no
On-Street Bike Lane	:	no
Off-Street Bike Path	:	no
Street Trees	:	50' o.c.
Curb Radii	:	25'

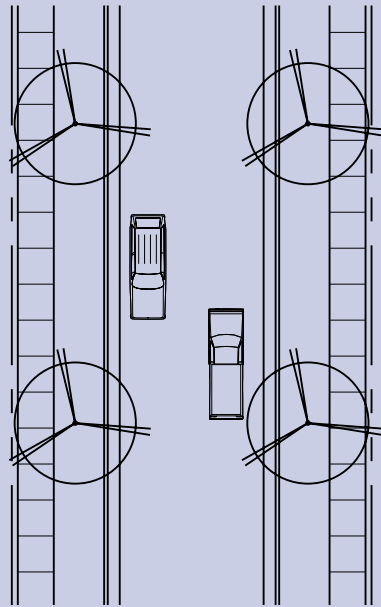
## Description

Route 29 and 141 are major truck routes that run through the Business Park, most importantly through the proposed Village Center. Uses fronting this street are retail commercial and office. Future traffic demand will likely require a four-lane divided highway.



*Location Map*



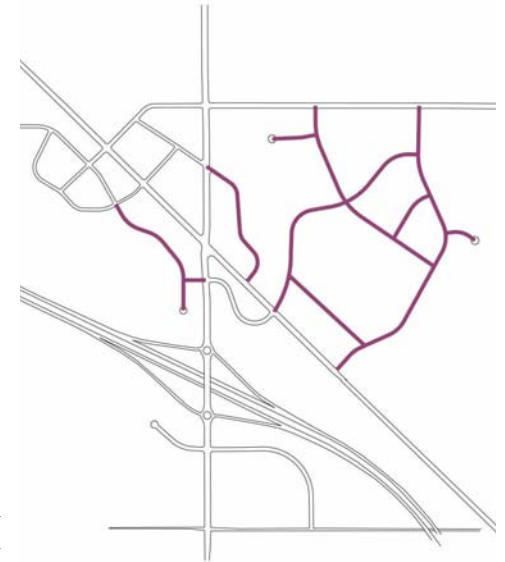


## Design Characteristics:

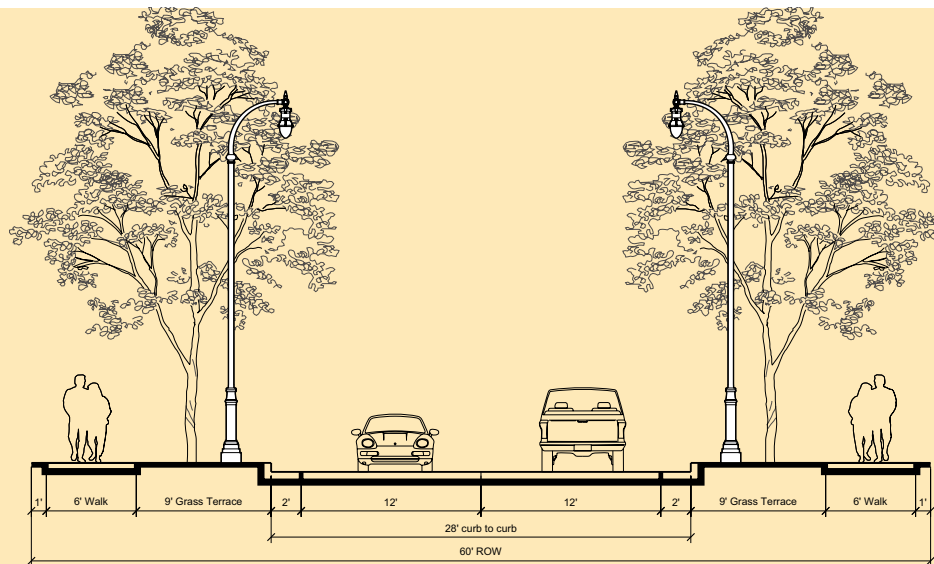
Right of Way	:	60'
Posted Speed	:	25 mph
Street Width	:	28'
Sidewalk	:	6'
Terrace Width	:	8'
Parking	:	no
On-Street Bike Lane	:	no
Off-Street Bike Path	:	no
Street Trees	:	50' o.c.
Curb Radii - Lt. Ind.	:	30'
Curb Radii - Big Box	:	25'

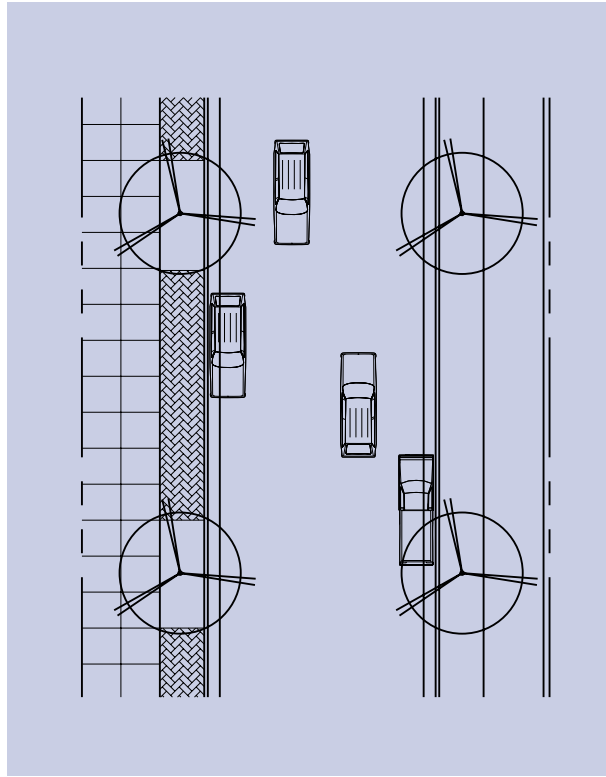
## Description

This street caters to a steady volume of traffic during the day, either in the form of pick up and delivery trucks to the various warehouses and industrial uses or in the form of cars shopping at the retail area. Sidewalks must be on at least one side, on both sides wherever feasible.



*Location Map*



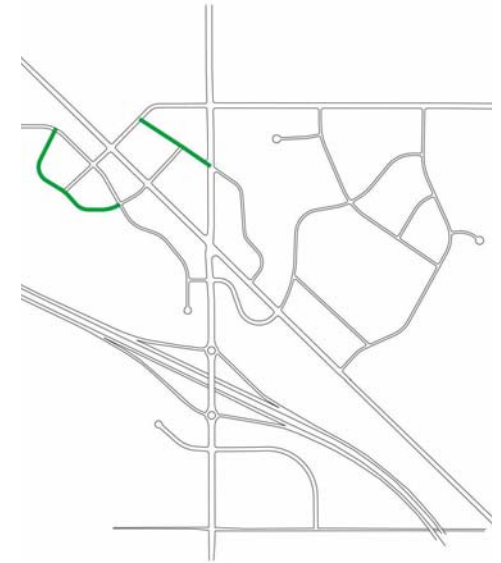


## Design Characteristics:

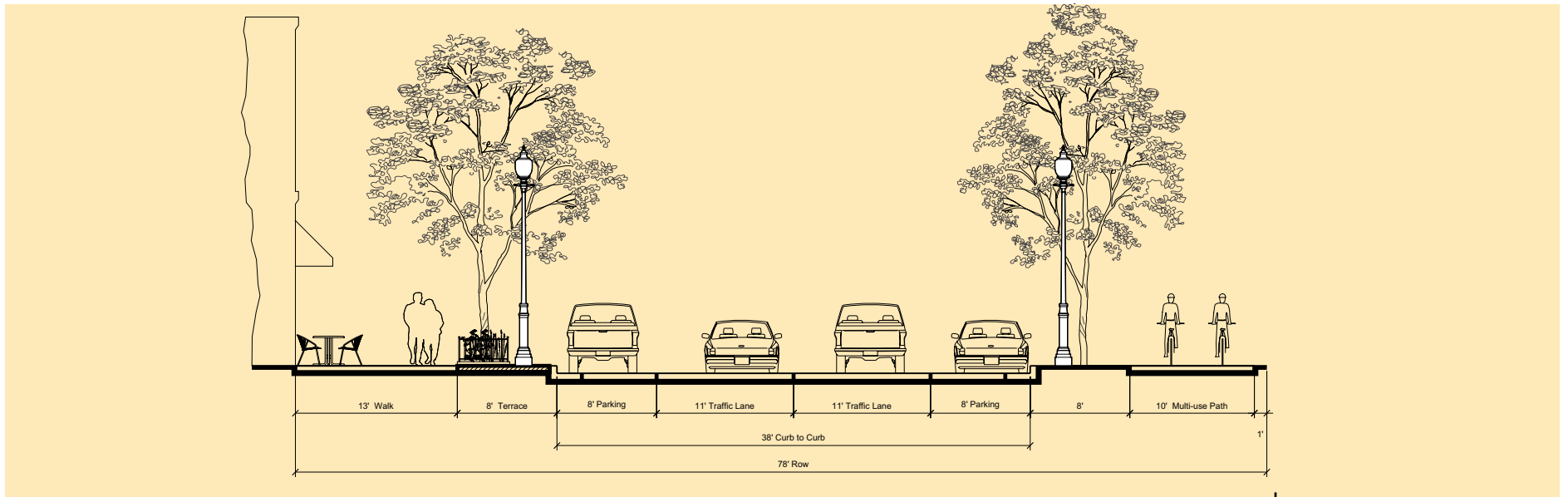
Right of Way	:	78'
Posted Speed	:	25 mph
Street Width	:	38'
Sidewalk	:	13'
Terrace Width	:	8'
Parking	:	yes
On-Street Bike Lane	:	no
Off-Street Bike Path	:	yes
Street Trees	:	50' o.c.
Curb Radii	:	15'

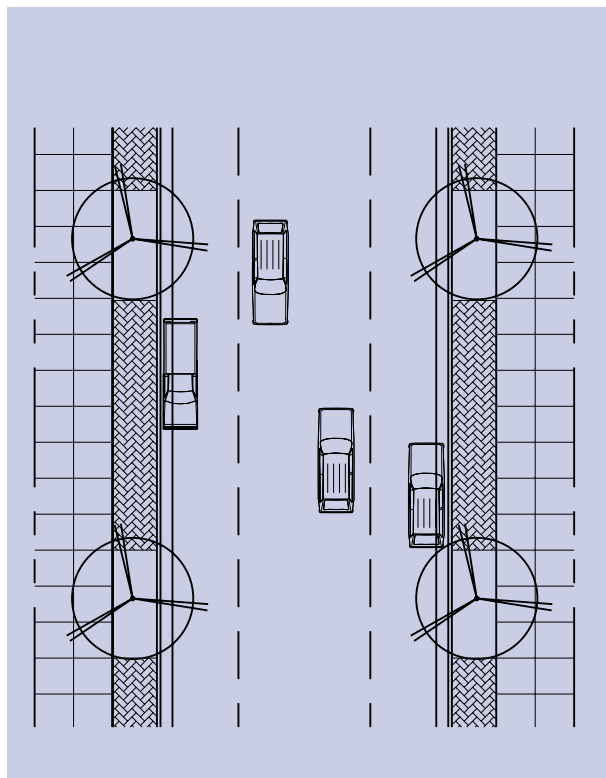
## Description

There are two types of streets in the Village Center. The first type shown here contains landscaped sidewalks and two lanes of traffic with two on-street parking lanes. At intersections, parking is replaced by bump outs. Sidewalks shall be wide enough to accommodate outdoor retail activities such as outdoor eating, street vendors etc. Bike paths are provided off-street.



Location Map



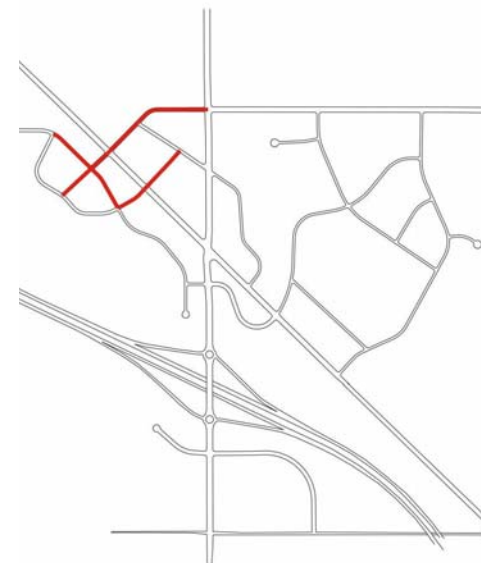


## Design Characteristics:

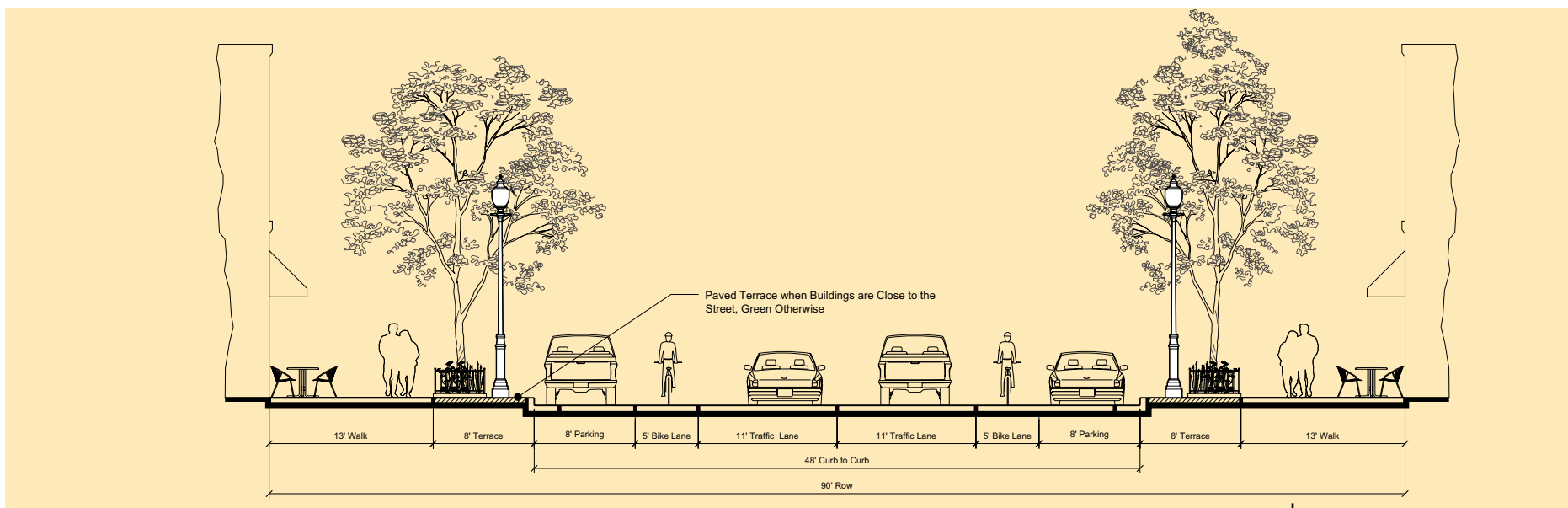
Right of Way	:	90'
Posted Speed	:	25 mph
Street Width	:	48'
Sidewalk	:	13'
Terrace Width	:	8'
Parking	:	yes
On-Street Bike Lane	:	yes
Off-Street Bike Path	:	no
Street Trees	:	50' o.c.
Curb radii	:	15'

## Description

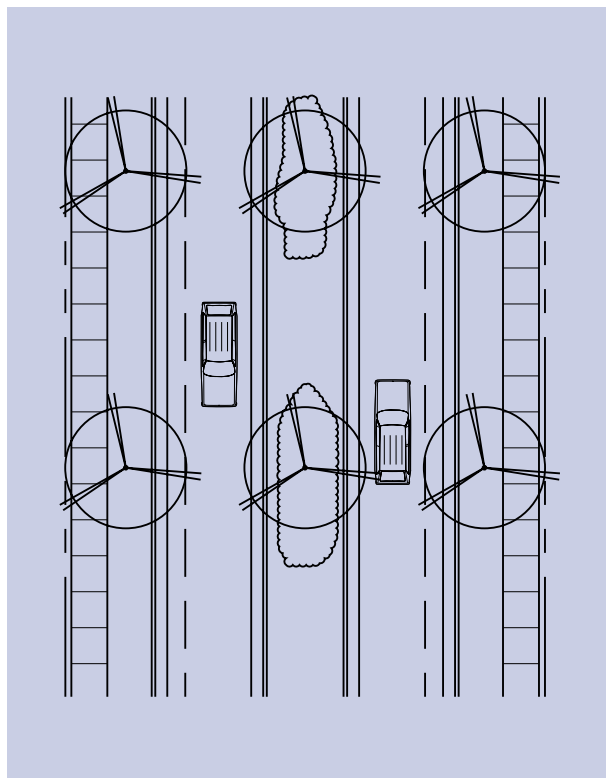
The second type of street is the central corridor coming into the Village Center. Bike traffic is accommodated on-street. At intersections with Route 141 and the Eastern Arterial, on-street parking is removed and replaced with a left turn lane. Use of landscape elements such as street furniture, trees, planter beds, street lighting and textured pavement shall enhance the overall experience and ambience of the Village Center.



*Location Map*





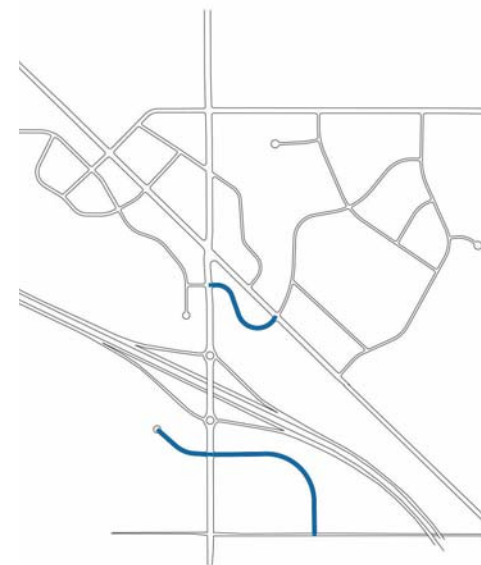


## Design Characteristics:

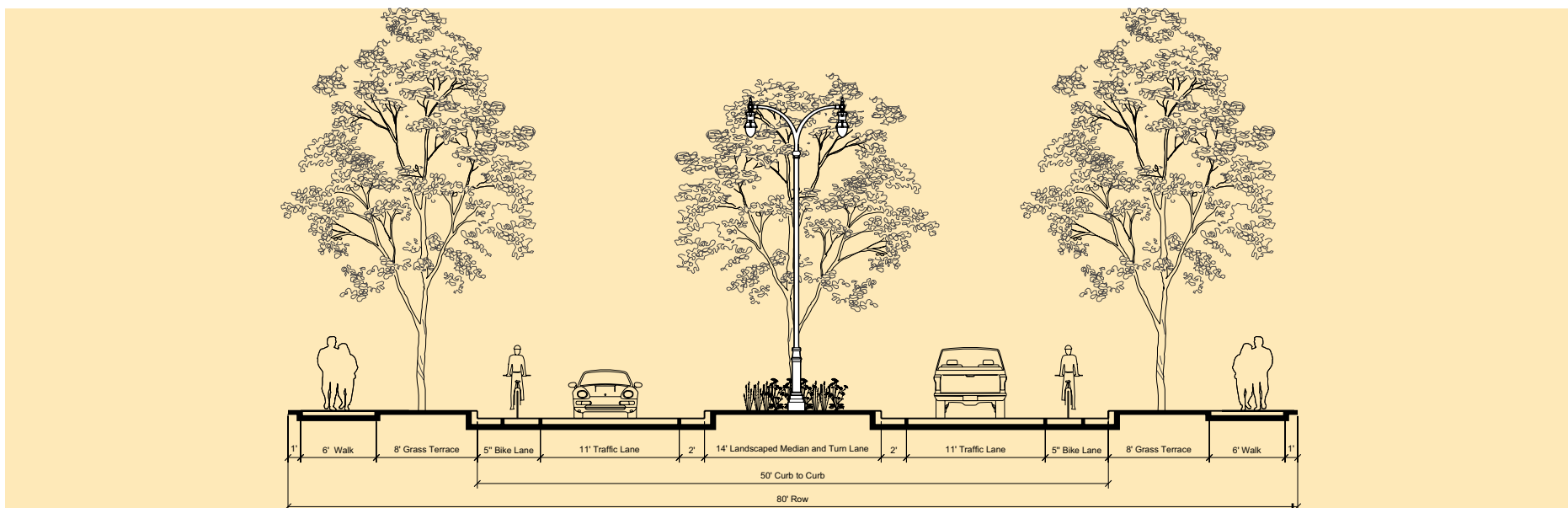
Right of Way	:	80'
Posted Speed	:	25 mph
Street Width	:	50'
Sidewalk	:	6'
Terrace Width	:	9'
Parking	:	no
On-Street Bike Lane	:	yes
Off-Street Bike Path	:	no
Street Trees	:	50' o.c.
Curb Radii	:	20'

## Description

The Corporate Commercial cross-section has two lanes, divided by a landscaped median. The median will be an opportunity to create a grand entrance to the corporate commercial building, as the roadway winds through open space and landscaped storm water ponds. A grass terrace and sidewalks flank the street.



Location Map



# **The Town of Ledgeview Neighborhood Center District**



Locust Town Center, Mt. Holly, NC

## **Concepts for Development**

Brown County Planning Commission  
100 North Jefferson Street  
Green Bay, WI 54301-5026  
[www.co.brown.wi.us/planning](http://www.co.brown.wi.us/planning)  
Adopted February 16, 2006

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## THE TOWN OF LEDGEVIEW NEIGHBORHOOD CENTER DISTRICT

A Neighborhood Center District serves as the focal point of a Neighborhood that contains retail, commercial, civic, and public services to meet the daily needs of community residents. The Neighborhood Center District is pedestrian-oriented and is designed to encourage pedestrian movement between residential, retail, and commercial uses. The Neighborhood Center District should generally include a public square for public gathering, and be located near significant intersections to provide access for visitors and residents alike. The Neighborhood Center District uses include small-scale retail shops, restaurants, offices, banks, hotels, post office, governmental offices, churches, community centers, and residential dwellings.



Olde-School Square Master Plan, Ledgeview WI



## Definitions

As used within this Picture Book, the following terms have the meanings indicated:

**Amenity** – Aesthetics or other characteristics of a development, such as unified building design or recreational facilities, that increase its desirability to a community or its marketability to the public.

**Banner** – See Temporary Signs.

**Bike Lane** – A designated area within a street roadway reserved for bicycle travel and separated from the roadway by painted lines or other pavement markings.

**Block** – An area of land with more than one use that is bounded by streets, roads, alleys, or rear property lines.

**Build-To Line** – The required distance from the right-of-way to a structure.

**Conservation Area** – Any parcel or area of undeveloped land conserved in its natural state for perpetuity through deeds or other legal means.

**Greenspace** – The total gross undeveloped areas of a property utilized for landscaping and conservation areas.

**NCD** – Neighborhood Center District. An area that provides for a variety of pedestrian-oriented retail, office, lodging, residential, and civic land uses in the town center area. The intent of an NCD is to encourage the development and sustainability of a vibrant mixed-use area where residents and visitors can live, work, shop, dine, be entertained, enjoy community and cultural events, and contribute to the economic viability of the Neighborhood Center District and the Town as a whole.

**Pennants** – See Temporary Signs.

**Permanent Signs** – A sign attached to a building, structure, or the ground in some manner that requires a permit from the Town of Ledgeview and which is made of durable materials intended to exist for the duration of time that the use or occupant is located on the premises.

**Preservation Area** – An area in which beneficial natural uses in their present condition are protected. For example, a nature preserve.

**Reserve-Parking Area** – A designated expansion area of a parking lot that is kept as a natural greenspace until such time that the parking area is needed and developed or determined unnecessary and developed for another use that is approved by the Town of Ledgeview.

**Sidewalk** – A paved surface or leveled area separated from the street and used as a pedestrian walkway.

**Special Use Permit** – A permit issued by the Town of Ledgeview that must be acquired before a special exception use can be constructed.

**Staff** – The Ledgeview Town Clerk, Town Engineer, and Town Zoning Administrator or their designees.

**Temporary Signs (Banners, Pennants, etc.)** – A sign intended to be hung either with or without a frame and that possesses characters, letters, illustrations, or ornamentations applied to paper, plastic, or fabric of any kind but shall not be interpreted to include a canopy sign. National flags, state or municipal flags, or the official flag of any institution or business shall not be considered a banner.

**Town Plaza** – A public open space at ground level wholly or partially enclosed by a building or buildings designed specifically for use by people as opposed to serving as a setting for a building.

**Trail** – A public access designed for use by pedestrians and non-motorized vehicles, such as bicycles.

**Travelway** – A private drive or public/private street within a NCD development

**Wetlands** – Areas that are inundated or saturated by surface or groundwater at a frequency and duration sufficient to support vegetation and/or wildlife typically adapted for life in saturated soil conditions, as defined by the Wisconsin Department of Natural Resources. Wetlands may include swamps, marshes, bogs, wet soils, waterways, and similar areas.

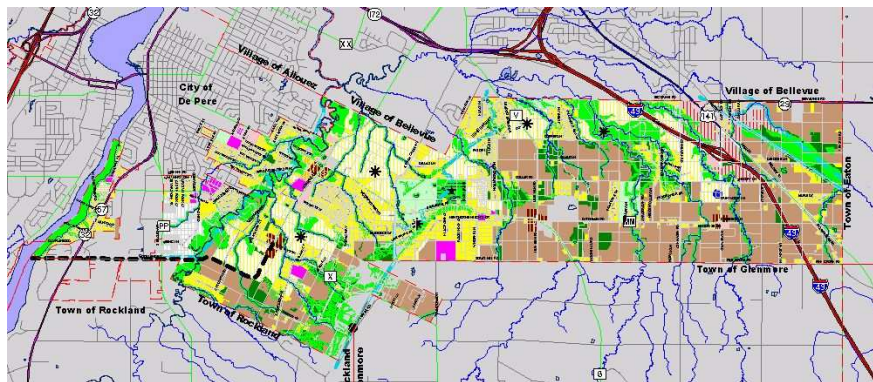
**Zero Lot Line** – The location of a building on a lot in such a manner that one or more of the building's sides rest directly on a lotline without overhanging the propertyline and it complies with all fire code requirements for construction on a lot line.

## The Purpose and Intent

The Neighborhood Center District is an important step towards achieving the Town of Ledgeview's vision for a Neighborhood Center that provides a variety of pedestrian-oriented retail, office, lodging, residential and civic land uses. The Neighborhood Center District encourages the development and sustainability of a vibrant mixed-use area where residents and visitors can live, work, shop, dine, be entertained, enjoy community and cultural events and contribute to the economic viability of the Neighborhood Center District and the Town of Ledgeview as a whole.

The Neighborhood Center District designation should be applied to land that meets the following primary characteristics:

- This district will be appropriate in areas identified as "Future Neighborhood Center" on the future land use map of the Town of Ledgeview Comprehensive Plan. Areas include the CTH G & CTH GV, CTH GV & CTH X, CTH G & Limekiln Road, and CTH MM & Elmview Road intersections.
- This district will be appropriate in areas of the Town of Ledgeview where commercial, lodging, residential and civic activities are desirable for the benefit of the residents and the Town in general.
- This district will be appropriate where its permitted uses and performance standards serve to widen the Town of Ledgeview's economic base and further the development of the Neighborhood Center as recommended in the Neighborhood Center District: Concepts for Development and the Town of Ledgeview Comprehensive Plan.



2004 Town of Ledgeview Future Land Use Plan

## **The Goals**

The Neighborhood Center District seeks to change uncontrolled development to a compact and interconnected design at a pedestrian scale. The following goals are sought in the Neighborhood Center District:

- Accommodate pedestrians, bicyclists, and public transportation so that mobility can be a reality for the elderly, the young, and encourages the transportation other than personal automobiles.
- Makes open space integral to overall design so that residents and workers can walk to a public park, experience preserved areas, and enjoy public gathering places.
- Keeps buildings and spaces at a human scale so that street views are attractive and pedestrian friendly.
- Contains a mixture of residential and non-residential uses so residents have convenient access to work, to services, and to entertainment.
- Requires interconnected streets within developments and between developments so that pedestrians can walk easily to many destinations, traffic has alternative routes, and car trips are reduced in number and length.
- Moves off-street parking out of sight and encourages parking on local and private streets.
- Mixes housing types and markets so that housing choices are offered within the NCD and adjacent neighborhoods.
- Emphasizes re-use of sites.
- Adapts development to the site terrain so that natural topography can be preserved.
- Maintains a clear boundary between developed areas and rural areas.
- Provides for neighborhoods to have a designated center to bring diverse and continuous activity to a neighborhood.



## The Principles

### Pedestrian Orientation

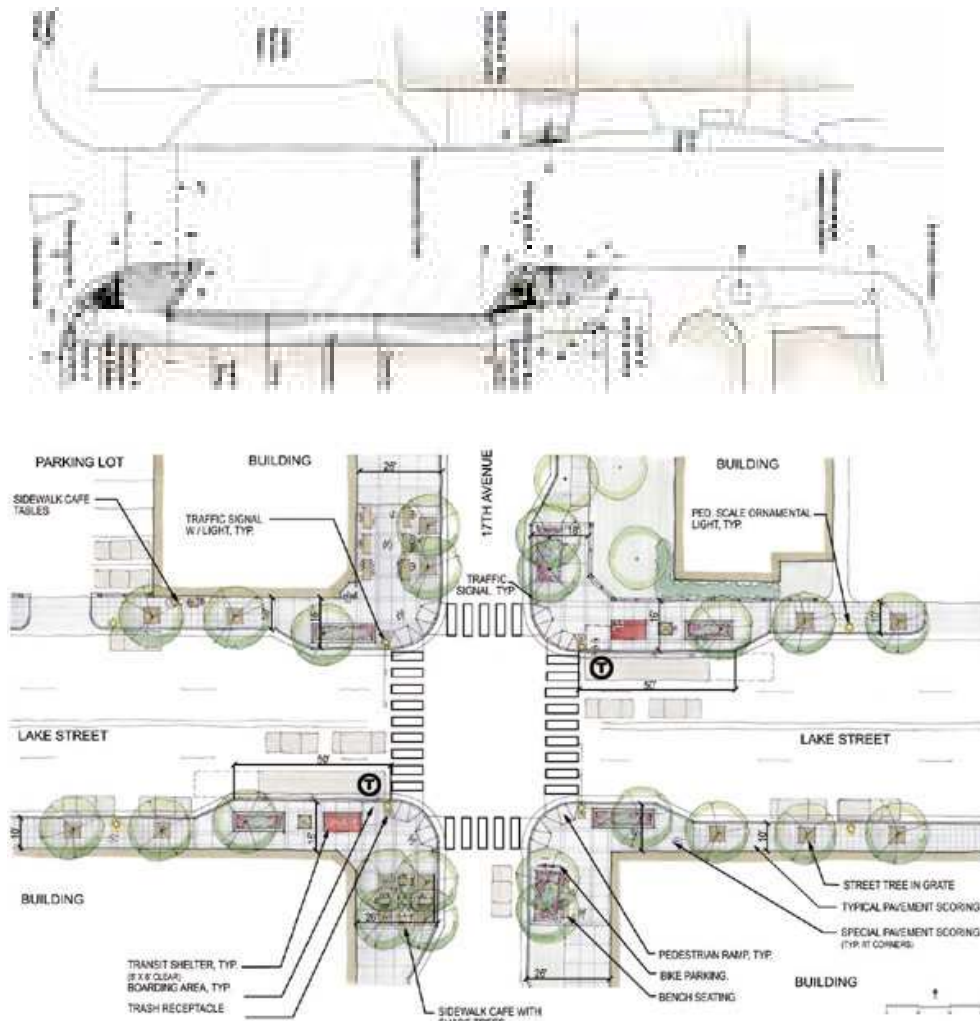
The Neighborhood Center District proposes a walkable community, with sidewalks and paths provided in new development and a variety of destinations within a 5-minute walk for residents. Pedestrian orientation requires a high degree of coordination among the other principles. All elements – density, building placement, street and path connections, and mixture of uses – must be designed with pedestrians in mind. The results should be a walkable community that reinforces all of the other goals for the neighborhood.



- Pedestrian comfort can be enhanced by careful attention to building design, location, setbacks, and orientation.

## Neighborhood friendly streets, sidewalks, paths and utilities

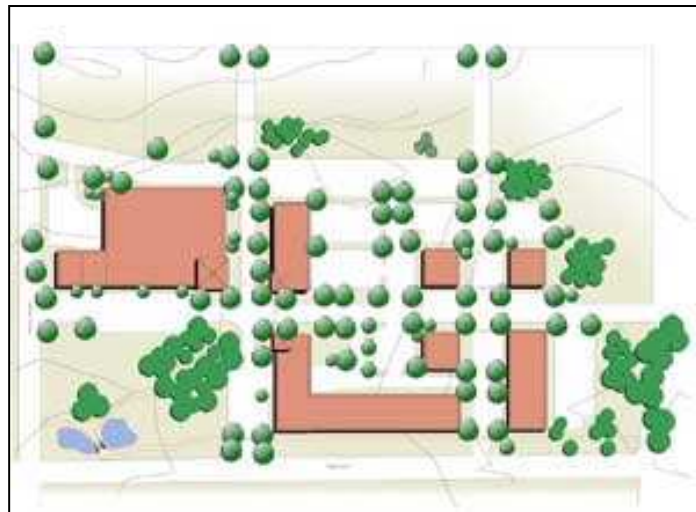
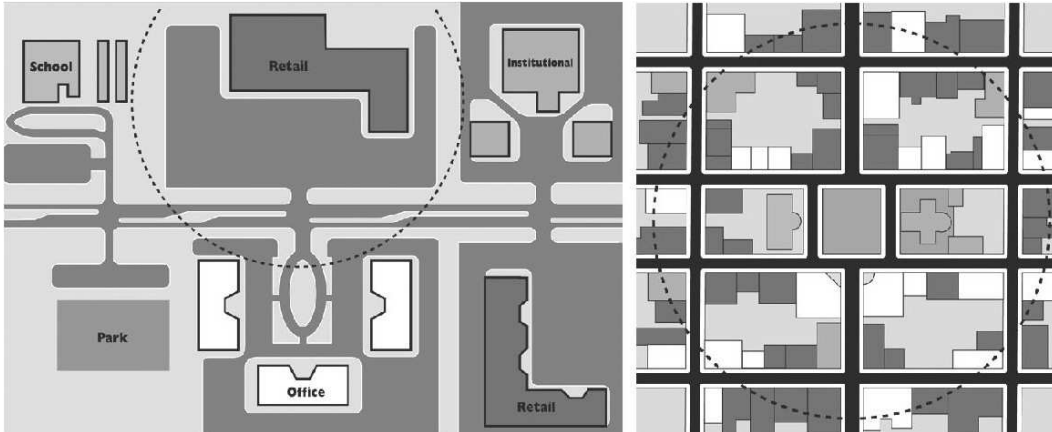
The Neighborhood Center District proposes that road design addresses character and appearance as well as design speed and capacity. Specifically, streets should be narrower, include streetscape elements such as street trees, provide paths for pedestrians and bicyclists, allow better distribution of traffic, and accommodate potential public transportation facilities. These changes would make transportation routes work better for all citizens, not just those in cars.



- The configuration of street and pedestrian facilities varies, and is defined primarily by the overall volume of travelers in the area. Traffic calming and street safety must be addressed.

## Interconnected streets and transportation networks

The Neighborhood Center District calls for a distributed network of streets within new developments, and connections with existing development. This network is essential to support the pedestrian environment, mixed-use developments, and compact, livable neighborhoods.



Distributed grid pattern streets are the design of choice.

- Distributed street and transportation networks attract development to the centers by offering more access and visibility than available anywhere else in the surrounding area.

## **Parks and open space as amenities**

The Neighborhood Center District proposes making open space integral to overall community design. This should result in residents having convenient access to parks, public gathering places, and natural views whether they are at home, work, or play. The open space system should provide an important framework for the organization of development. It should serve as a connective element to join different development areas, in addition to preserving important aspects of the natural environment.

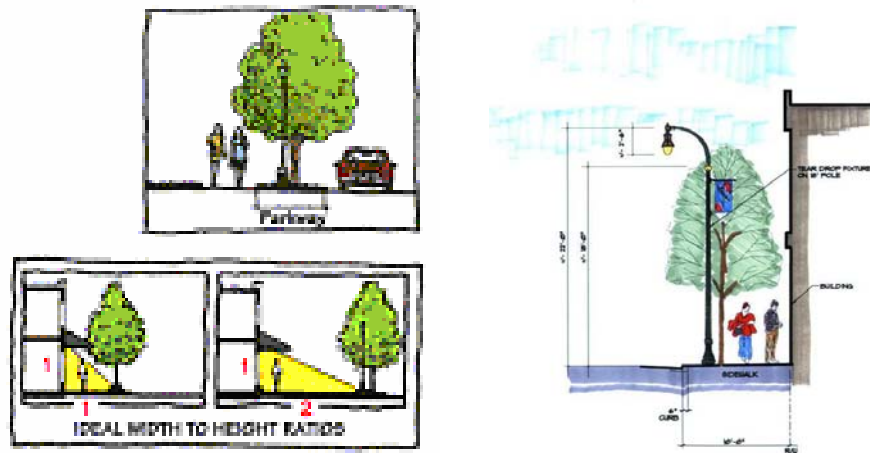


- Parks and open space provide gathering areas for a variety of seasonal uses.



## Buildings and spaces of human scale

The Neighborhood Center District proposes the creation of livable urban areas, buildings and spaces that are attractive and inviting. The elements of building height, setback, yards, architecture, and special enclosure all contribute to the appearance and function of a place. When proportionate to the human body and considered in their context, buildings and spaces of human scale improve the quality of a place.



- Four examples of width-to-height ratios that maintain a human scale.



- Canopies over this sidewalk on a two-story building create an ideal ratio.

## Relegated shared parking

The Neighborhood Center District proposes reassessing parking standards and finding ways to make parking areas less dominant from the street, and allowing for bicycle parking. Such steps should improve the visual character of the community and make possible a more functional and appealing pedestrian environment.

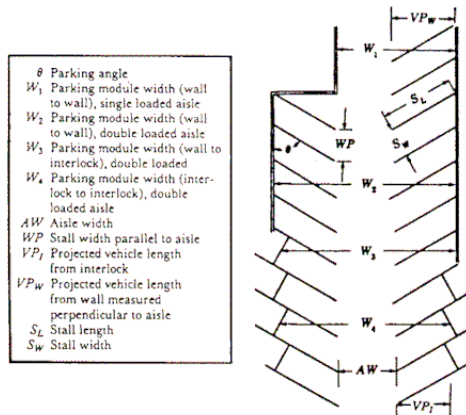


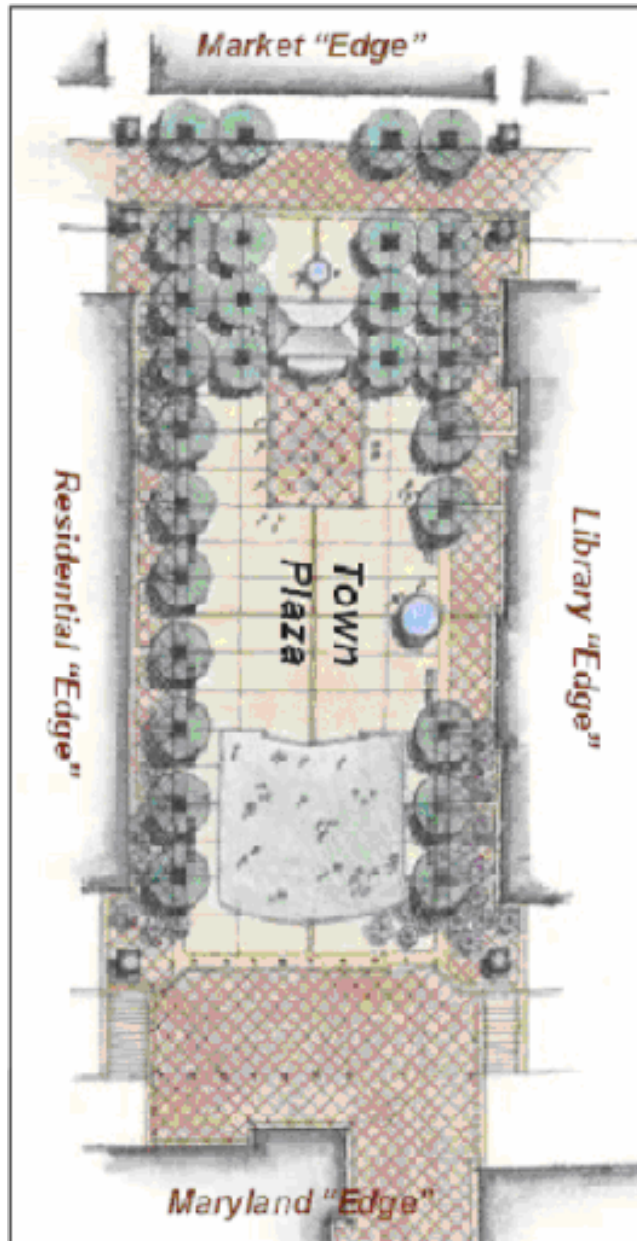
Figure 8.4. Dimensional elements of possible parking layouts



- Relegated parking emphasizes and allows for clusters of work-shopping-residential, and walkability and open spaces instead of suburban sprawl.

## Mixture of uses and use types

The Neighborhood Center District proposes and encourages compatible uses side-by-side. This mixture is the key to the walkability of the neighborhood and to achieving increased densities.



- The mixed uses complement and enhance each other, avoiding the creation of directly adjacent competition, providing opportunities to visit several areas in a walkable distance.

## Redevelopment

The Neighborhood Center District emphasizes reuse of existing sites and buildings. This strategy encourages efficiency the use of land and infrastructure, relieving pressure on rural areas and maintaining value of existing neighborhoods



- Redeveloping existing structures preserves the historic feel and integrity of a site.



### **Site planning that respects terrain**

The Neighborhood Center District proposes sensitivity to existing terrain through the site design and architecture that fits into grades. Where significant regrading is necessary, reconstructed slopes are called for that are attractive, functional, and easy to maintain. Minimizing destruction of natural terrain will help retain beautiful landscapes.



### **Clear boundaries with rural areas**

The Neighborhood Center District proposes maintaining the existing borders to the development areas and having distinctive boundaries between the developed areas and the rural areas. This principal increases the incentives to build efficiently while minimizing threats to rural areas.



## **The Rules of Development**

The “Code of Development” within the Neighborhood Center District Article of the Zoning Ordinance establishes the unifying design guidelines, permitted uses, and the specific regulations for the district. The “Rules of Development” within this document offers a flexible range of design concepts to achieve the use characteristics of each block, appearance of central features, and a mix of uses and densities.

To address these design concepts, each project shall address the following elements, and utilize the following suggested concepts and rules of development:

## Mixed Uses

The way in which uses are placed on a site creates impacts that can be mitigated to allow for office, retail, civic, and even residential uses in close proximity.



Photographs, elevations, and site plans identify the relationship between the mixed uses, pedestrians, sidewalks, and street features.

- Each district shall have at least two different general use classifications.
- The mixture of land uses shall be based upon the uses recommended in the Ledgerview Comprehensive Plan.
- The required mixture of uses may be obtained with different uses within the same building.

## Architectural Styles

The Neighborhood Center must retain a consistent theme, or architectural style. Approximate building sizes, building materials, colors, and lighting must coincide with the style chosen. For example, if the center is designed to reflect a late 1800's Main Street, any future additions or modifications to the center must also reflect similar features.



Renditions and elevations identify the breakup of building mass.



Renditions and elevations identify the relationship between existing and future structures.



- Architectural styles shall be at a human scale, breaking up structural mass.
- All sides of structures shall have a finished appearance (no widows and doors that appear to function (no blank walls)).
- Rooftop and ground equipment shall be screened and integrated into the building design.

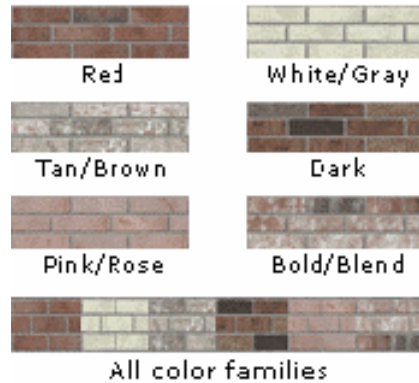


## Materials, Colors, and Textures

The design must present an appropriate combination of materials, colors, and textures that reflect the architectural style chosen. Materials that are long lasting, and easily maintained should be considered.



Tumbled and split face block.



Brick.

**The mixture and relationship between material color and types are clearly identified.**



Textured Concrete.



Wood.

**Elevations, photos, and renditions clearly identify the specific color and types of materials used.**

- Material combinations, such as stone, brick, painted metal, and wood are encouraged.
- Vinyl and unfinished (standard gray) split faced block are prohibited.
- EIFS is limited to dormers, gables, and soffits only.

## Roof Form and Pitch

The design must present an appropriate combination of roof lines and pitch. Roof forms must be continuous and wrap around corners and building planes.

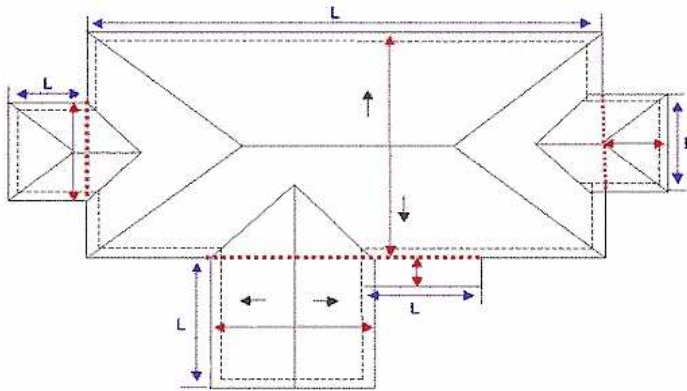


Angled roof.

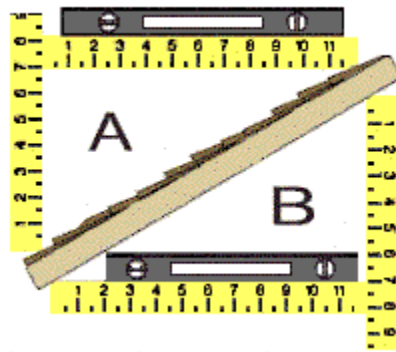


Flat roof.

Roofline relationships for existing and proposed buildings are identified in photographs and elevations.



The pitch in this example is 7/12

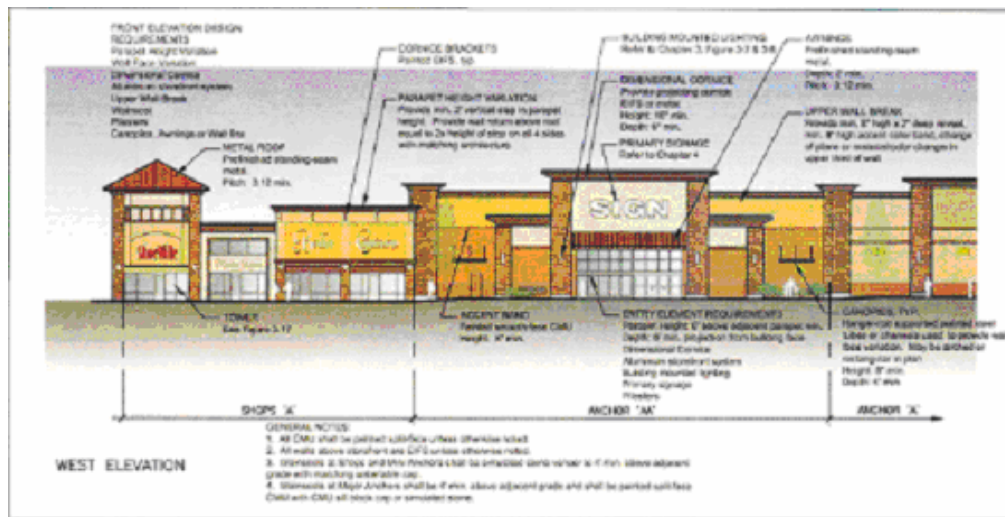


Roofline pitch is clearly labeled on site plans and elevations for compliance with NCD.

- Rooflines shall be articulated.
- Flat roofs are allowed provided the architectural theme of the Neighborhood Center justifies flat roof structures.

## Architectural Ornamentation

Finials, scalloping, weather vanes, wood shingle roofs, columns, cupolas, outdoor dining patios, ornamental fences, etc., should be considered in the Neighborhood Center Theme. Like color, materials, and architectural style, the ornamentation should be consistent throughout the Neighborhood Center.



Ornamentation creates a consistent identity for the NCD site.



Consistency can be easily displayed in colored elevations and renditions.

- Architectural ornamentation shall be labeled on the site plan
- Ornamentation features shall be used to create focus points within the Neighborhood Center.

## Façade Treatments

Windows, doors, shutters, awnings, entry coverage, and wall & awning signage must be designed in a theme consistent with the Neighborhood Center. All materials used must be identified in the plan, and a combination of materials is recommended. Façade treatments should be used to create unique features that create an identity for the Neighborhood Center.



**Various façade treatments can create an exclusive identity for an individual structure that matches other structures in the NCD.**



- Elevations shall use a combination of masonry, natural stone, rusted steel (in special situations), painted steel, and exposed wooded beams and columns, consistent with the theme of the Neighborhood Center.
- Sills, windows, leaders, and other features shall be used for visual articulation.
- Elevations shall utilize more than one material in the face of the elevation, excluding windows.



## Preservation of Historic Structures

Historic structures, sites, and archeological sites identified by the Wisconsin Historical Society, or recognized by the Town of Ledgeview should be preserved.



**Clearly identify structures targeted for preservation.**

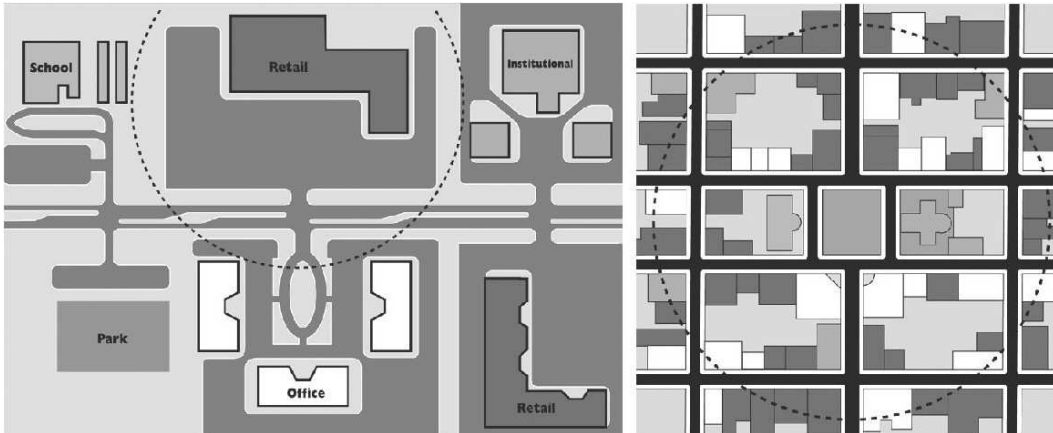


**Identify intentions for renovation, particularly if a historic structure qualifies for exemptions from certain NCD requirements. All exemptions must be approved.**

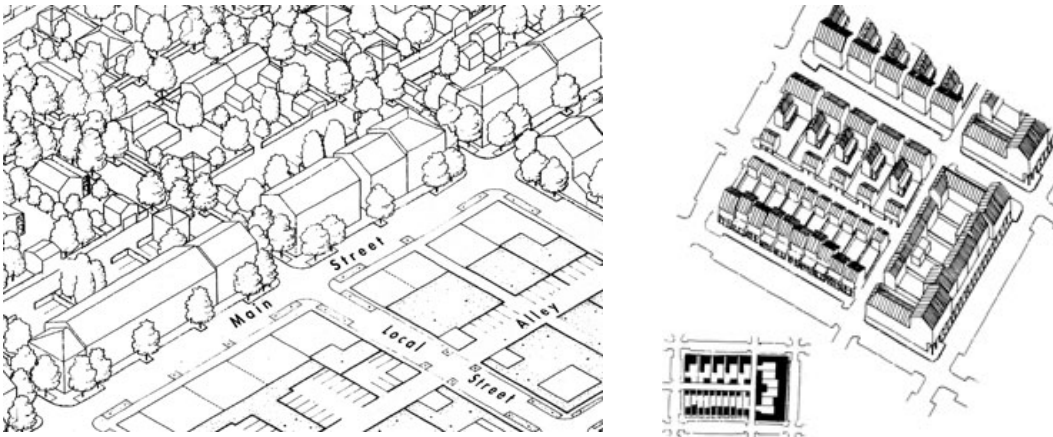
- Exemptions may be allowed from certain design criteria in order to maintain the integrity of existing historic structures and sites.
- Historic structures should be utilized as an “architectural model” for new structures within the Neighborhood Center.
- Historic structures and sites should be utilized as focus features.

## Blocks and Lots

The purpose for blocks and lots is to establish a logical layout for development, and to ensure that adequate pedestrian, bicycle, and vehicular traffic is available for the site.



**Clearly define pedestrian, bicycle, and vehicular access capability between lots and blocks.**

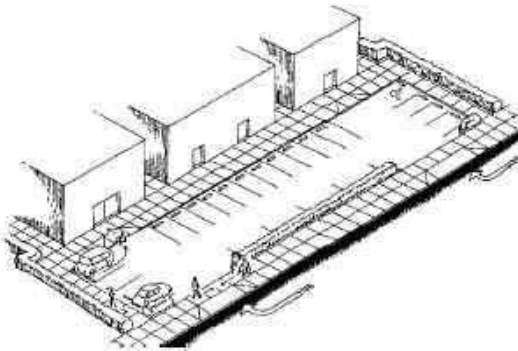


**This lot and block layout demonstrates access with vehicle parking on the side and rear of buildings.**

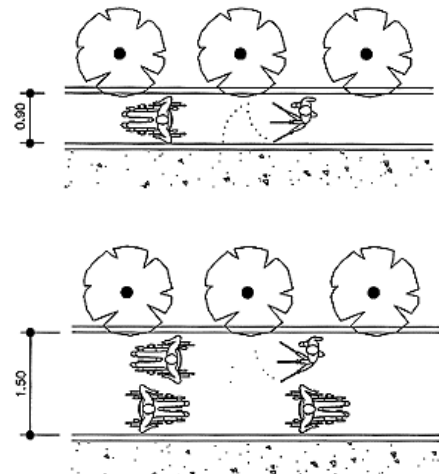
- A minimum of two uses per lot is required.
- Lots (property lines) shall be designated.
- Placement of buildings shall be clustered, with a focus to include pedestrian access within clustered buildings and between clusters.
- Front yards shall face or be visible to a street.

## Sidewalks and Pedestrian Paths

Hardscape areas should utilize integral color, which are compatible with the proposed development. Materials can include stamped concrete, cobblestone simulation, and colored concrete. The use of exposed aggregate as an accent within sidewalks and plazas are encouraged.



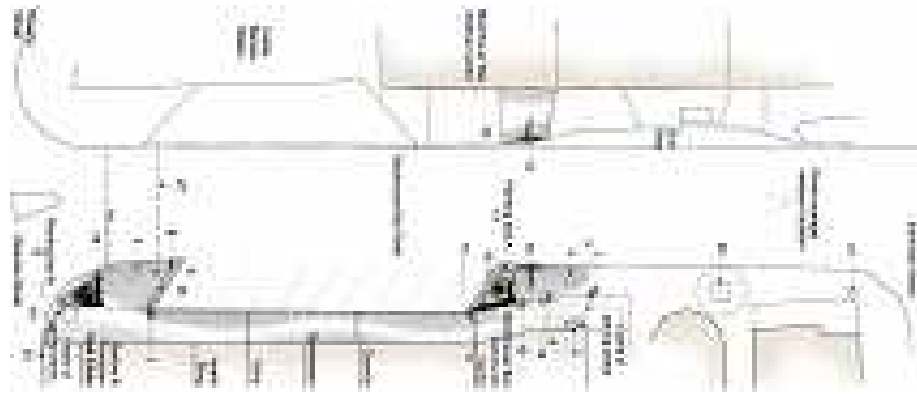
**Clearly define pedestrian connectivity, dimensions, and construction materials throughout the layout from multiple perspectives, not just on a site plan.**



- Sidewalks are on the front/main entry of buildings.
- Sidewalks shall be 6-10 feet wide to encourage walking.
- Accommodations for pedestrian/bicycle paths connecting to adjacent/future residential neighborhoods and/or a parallel trail shall be included in the design.

## Streets

Street right-of-way width and designs are described in the summary table below. On street parking and bicycle lanes are encouraged in certain areas.



Clearly define street connectivity, dimensions, and construction materials throughout the layout from multiple perspectives, not just on a site plan.

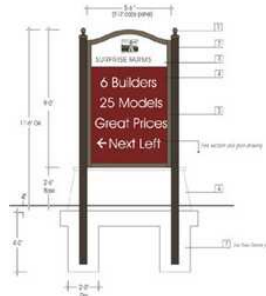


The relationship between streets, sidewalks, bike lanes, and pedestrian ways are is clearly displayed above.



## Signage

Neighborhood Center signage is intended to encourage attractive signs for business and services while promoting the general welfare of the community by creating aesthetically appealing street frontages.



The use of photographs and scaled graphics clearly describe the type and appearance of various signs. Accommodating text describes materials, illumination, and size details.

- Signage shall utilize the existing sign ordinance and the requirements of the NCD ordinance.
- Special allowances have been established for individual signs, directory signs, monument signs, banners, pennants, and temporary signs.

## Lighting

All lighting shall be pedestrian level at a height no greater than 20 feet, not including county highway illumination. The illumination of pedestrian walkways can be any combination of lighted bollards, recessed light fixtures, or wall mounted fixtures. Parking lot lighting shall be a maximum of 50-feet in height above grade.



Lighting details and photographs clearly define pedestrian and parking lot lighting consistency with building and street design.



- Freestanding (Christmas string) light standards are discouraged.
- Light bulb elements shall match throughout the Neighborhood Center.
- Light pole color and design shall be compliant with building architecture and sidewalks.

## Green Space & Amenities

Each Neighborhood Center should contain at least 15% of the area devoted to green space and 15% of the area devoted to amenities. The preservation of existing wetlands, and the use of Wisconsin prairie grasses in areas that are not yet developed are recommended.



Greenspace can be small individual areas or large natural areas.



Amenities display locations to be utilized by the public. Display how the spaces can be used.



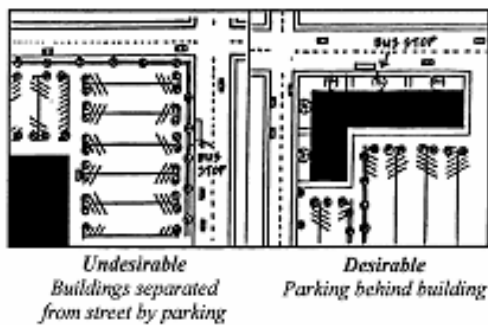
- An applicant may request a reduction in the greenspace and amenity percentages if the reduction better achieves the neighborhood model goals of the Ledgeview Comprehensive Plan.



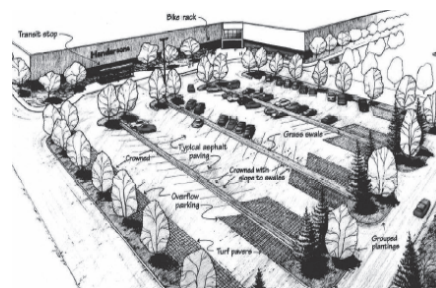


## Parking Areas

All parking stalls are shared throughout the Neighborhood Center. Parking areas should accommodate bicycle parking at a point conveniently close to the pedestrian/sidewalk areas and main entry of buildings. The primary entry of buildings should accommodate limited parking to encourage pedestrian activity, yet allow short-term parking conveniences.



Display access to parking areas, and how the areas are screened from pedestrian and public view.



Identify vehicular and bicycle parking areas. As well as landscaping and connections to sidewalks.

- Parking areas should be primarily at the sides and rear of buildings, partially, or completely screened from public right-of-ways.
- Parking areas require 10% greenspace to break up the mass of large parking lots.

## Civic Spaces

Civic spaces such as schools, libraries, government offices, places of worship, and their associated yards should be developed for community and civic activities.



**The location and uses for civic spaces, similar to amenities, are identified. Access to other public and private areas are also clearly identified.**

- Civic spaces encourage a regular flow of public entering and exiting the Neighborhood Center.

## Implementation

The Neighborhood Center District is an important step towards achieving the Town of Ledgeview's vision for a Neighborhood Center that provides a variety of pedestrian-oriented retail, office, lodging, residential and civic land uses. The Neighborhood Center District encourages the development and sustainability of a vibrant mixed-use area where residents and visitors can live, work, shop, dine, be entertained, enjoy community and cultural events and contribute to the economic viability of the Neighborhood Center District and the Town of Ledgeview as a whole.

The following three steps are required to implement the Neighborhood Center District Model:

1. **Modify Zoning Ordinance and Zoning Map.** The current Zoning Ordinance should be amended to include the Neighborhood Center District section and the supporting Neighborhood Center District document. Also, the Neighborhood Centers identified in the Comprehensive Plan should be rezoned on the Town of Ledgeview Zoning Map.
2. **Encourage the development of Master Plans for the area.** The Neighborhood Center District provides a framework for private developers to design areas that will attract functional and appealing new growth to the area. The use of Master Plans will insure that public features such as street networks and public spaces connect in a compact, efficient manner.
3. **Create plans to develop infrastructure.** The Town of Ledgeview will have to arrive at a policy to address the expectations of developers for infrastructure to be designed simultaneous with to new development.

## Approval Process

The review and approval process for Master Plans in the Neighborhood Center District are outlined in the Zoning Ordinance. The process includes the approval process, information required, Special Use approval process, and amendments.

## References

Airpark Banner & Signs, Scottsdale, AZ

Auckland City, New Zealand

Broadway/MacArthur/San Pablo Redevelopment Project, Oakland CA

Brown County Planning Commission Staff

City of Charlottesville, NC

City of Dalles, OR

City of Green Bay, WI

City of Kogarah, Australia

City of Los Angeles, CA

City of Marietta, GA

City of St. Petersburg, FL

City of Pennsylvania, Penn

City of Rockville, Maryland

City of Tokyo, Japan

City of Vancouver, WA

City of West Palm Beach, FL

City of Woodlands, TX

County of Sacramento Regional Parks, Recreation & Open Space

Lamancha, Spain

Locust Town Center, Mt. Holly, NC

Middleton Hills, Madison, WI

Munich, Germany



Oakland County, CA

South Village, South Burlington, Vermont

Toledo, Spain

*Weant, R.A. and Levinson, H.S., Parking, Eno Foundation, 1990*

Wesley Battle Neighborhood Association

Wisconsin Public Service

## COST ESTIMATE



Project: **Ledgeview Business Park**

Project Number: 2134

Cost Estimate: **Streetscape Type Totals Summary**

Date: June 2006

Estimated by:bm

Approved by:gj

Totals	
Eastern Arterial	\$2,145,760
Regional Arterial	\$2,448,919
Light Industrial / Big Box Commercial	\$8,937,848
Village Center - Type 1	\$2,954,424
Village Center - Type 2	\$5,919,946
Corporate Campus	\$2,430,589
Trail Head Park	\$422,291
Business Park Amenities	\$994,275
<b>SUMMARY TOTAL</b>	<b>\$26,254,051</b>

Includes a 35% contingency

## COST ESTIMATE



Project: Ledgeview Business Park

Date: June 2006

Project Number: 2134

Estimated by:bm

Cost Estimate: **Eastern Arterial**

Approved by:gj

Item	Qty.	Unit	Unit cost	Item Total	Remarks
Street	20%	ls	\$2,052,540	\$410,508	Assume 20% of cost by born by City
Sidewalk	63,876	sf	\$4	\$255,504	5' wide
Roadway Light	54	ea	\$8,500	\$459,000	assume 120' o.c. and 2 per intersection
Roadway Light with signal Arm	8	ea	\$14,000	\$112,000	signal not included
Roundabout Entry Feature	2	ea	\$75,000	\$150,000	
Street Tree	177	ea	\$500	\$88,500	2.5"-3" caliper, assume 60' o.c.
Ornamental Trees	100	ea	\$350	\$35,000	
Median Planting	7,894	sf	\$10	\$78,940	assume 20% of median planted
Streetscape Subtotal				\$1,589,452	
35% Contingency				\$556,308	
<b>STREETSCAPE TOTAL</b>				<b>\$2,145,760</b>	Engineering, Design & Unknowns

## COST ESTIMATE



Project: Ledgeview Business Park

Date: June 2006

Project Number: 2134

Estimated by:bm

Cost Estimate: **Regional Arterial: 4-Lane**

Approved by:gj

Item	Qty.	Unit	Unit cost	Item Total	Remarks
Street	20%	ls	\$2,222,850	\$444,570	Assume 20% of cost by born by City
Sidewalk	95,196	sf	\$4	\$380,784	5' wide
Roadway Light	77	ea	\$8,500	\$654,500	assume 120' o.c. and 2 per intersection
Street Tree	265	ea	\$500	\$132,500	2.5"-3" caliper, assume 60' o.c.
Ornamental Trees	182	ea	\$330	\$60,060	
Median Planting	14,160	sf	\$10	\$141,600	assume 20% of median planted
Streetscape Subtotal				\$1,814,014	
35% Contingency				\$634,905	Engineering, Design & Unknowns
<b>STREETSCAPE TOTAL</b>				<b>\$2,448,919</b>	



## COST ESTIMATE



Project: Ledgeview Business Park

Date: June 2006

Project Number: 2134

Estimated by:bm

Cost Estimate: **Light Industrial Big Box Commercial**

Approved by: gj

Item	Qty.	Unit	Unit cost	Item Total	Remarks
Street	1	ls	\$2,559,000	\$2,559,000	
Sanitary Sewer	1	ls	\$1,108,900	\$1,108,900	
Water Main	1	ls	\$767,700	\$767,700	
Sidewalk	196,632	sf	\$4	\$786,528	5' wide
Roadway Light	130	ea	\$8,500	\$1,105,000	assume 150' o.c. and 2 per intersection
Street Tree	547	ea	\$500	\$273,500	2.5"-3" caliper, assume 60' o.c.
landscape Allowance	1	ls	\$20,000	\$20,000	entry areas
Subtotal				\$6,620,628	
35% Contingency				\$2,317,220	Engineering, Design & Unknowns
<b>STREETSCAPE TOTAL</b>				<b>\$8,937,848</b>	

## COST ESTIMATE



Project: Ledgeview Business Park

Date: June 2006

Project Number: 2134

Estimated by:bm

Cost Estimate: **Village Center - Type 1**

Approved by:gj

Item	Qty.	Unit	Unit cost	Item Total	Remarks
Street	1	ls	\$510,400	\$510,400	
Sanitary Sewer	1	ls	\$188,500	\$188,500	
Water Main	1	ls	\$130,500	\$130,500	
Street Tree	116	ea	\$500	\$58,000	Assume 50 o.c.
Pedestrian Light	72	ea	\$5,000	\$360,000	assume opposite 80 o.c.
Bollard	12	ea	\$1,000	\$12,000	at 6 crosswalks
Special Pavement Crosswalk	6	ea	\$3,120	\$18,720	8' x 26' @ \$15 sf
Specialty Pavement	18,368	sf	\$12	\$220,416	assume 8' wide on 1 side of street
Sidewalk	37,388	sf	\$4	\$149,552	assume 13' wide, 1 side of street
Multi-Use Path	2,876	lf	\$24	\$69,024	
Plaza	6,345	sf	\$30	\$190,350	
Cast-in-Place Planter	58	ea	\$4,000	\$232,000	32 lf of curb with decorative fence & plantings
Bench	10	ea	\$2,000	\$20,000	
Trash Receptacles	6	ea	\$1,500	\$9,000	
Landscape Allowance	1	ls	\$20,000	\$20,000	
Subtotal				\$2,188,462	
35% Contingency				\$765,962	Engineering, Design & Unknowns
<b>STREETSCAPE TOTAL</b>				<b>\$2,954,424</b>	

# COST ESTIMATE



Project: Ledgerview Business Park

Date: June 2006

Project Number: 2134

Estimated by:bm

Cost Estimate: **Village Center - Type 2 Streetscape**

Approved by:gj

Item	Qty.	Unit	Unit cost	Item Total	Remarks
Street	1	ls	\$1,063,575	\$1,063,575	
Sanitary Sewer	1	ls	\$294,125	\$294,125	
Water Main	1	ls	\$203,625	\$203,625	
Street Tree	183	ea	\$500	\$91,500	Assume 50 o.c.
Pedestrian Light	115	ea	\$5,000	\$575,000	assume opposite 80 o.c.
Bollard	30	ea	\$1,000	\$30,000	at 15 crosswalks
Special Pavement Crosswalk	15	ea	\$3,120	\$46,800	8' x 26' @ \$15 sf
Specialty Pavement	58,560	sf	\$12	\$702,720	assume 8' wide on both sides of street
Sidewalk	118,950	sf	\$4	\$475,800	assume 13' wide, BOTH side of street
Cast-in-Place Planter	183	ea	\$4,000	\$732,000	32 lf of curb with decorative fence and plantings
Bench	20	ea	\$2,000	\$40,000	
Trash Receptacles	10	ea	\$1,500	\$15,000	
Landscape Allowance	1	ls	\$40,000	\$40,000	
Entry Feature	3	ea	\$25,000	\$75,000	
Subtotal				\$4,385,145	
35% Contingency				\$1,534,801	Engineering, Design & Unknowns
<b>STREETSCAPE TOTAL</b>				<b>\$5,919,946</b>	

## COST ESTIMATE



Project: Ledgeview Business Park

Date: June 2006

Project Number: 2134

Estimated by:bm

Cost Estimate: **Corporate Campus Streetscape**

Approved by:gj

Item	Qty.	Unit	Unit cost	Item Total	Remarks
Street	1	ls	\$749,250	\$749,250	
Sanitary Sewer	1	ls	\$263,250	\$263,250	
Water Main	1	ls	\$182,250	\$182,250	
Sidewalk	48,504	sf	\$4	\$194,016	5' wide
Twin Roadway Light	23	ea	\$10,000	\$230,000	assume 150' o.c.
Street Tree	243	ea	\$500	\$121,500	2.5"-3" caliper, assume 60' o.c.
Median Planting	6,017	sf	\$10	\$60,170	assume 20% of median planted
Subtotal				\$1,800,436	
35% Contingency				\$630,153	Engineering, Design & Unknowns
<b>STREETSCAPE TOTAL</b>				<b>\$2,430,589</b>	



## COST ESTIMATE



Project: Ledgeview Business Park

Date: June 2006

Project Number: 2134

Estimated by:bm

Cost Estimate: **Trail Head Park**

Item	Qty.	Unit	Unit cost	Item Total	Remarks
Pedestrian Light	10	ea	\$5,500	\$55,000	
Gazebo	1	ls	\$100,000	\$100,000	
Kiosk	1	ea	\$15,000	\$15,000	
Drinking Fountain	1	ea	\$7,000	\$7,000	
Pathways - 10' wide	1,242	lf	\$24	\$29,808	asphalt, rail-to-trail path not included
Landscape Allowance	1	ls	\$20,000	\$20,000	
Site Amenities	1	ls	\$50,000	\$50,000	benches, trash receptacles, tables, bike racks
Parking lot	36	sp	\$1,000	\$36,000	
Park Subtotal				\$312,808	
35% Contingency				\$109,483	Engineering, Design & Unknowns
<b>TRAIL HEAD PARK TOTAL</b>				<b>\$422,291</b>	

## COST ESTIMATE



Project: Ledgeview Business Park

Date: June 2006

Project Number: 2134

Estimated by:bm

Cost Estimate: Business Park Amenities

Item	Qty.	Unit	Unit cost	Item Total	Remarks
Entry monuments	4	ea	\$40,000	\$160,000	
Wayfinding Signs	15	ea	\$1,000	\$15,000	Alum. Sheet, design N.I.C.
Bike Crossing Signals	2	ea	\$18,000	\$36,000	
Pathways - 10' wide, asphalt	11,625	lf	\$24	\$279,000	Park and rail-to-trail paths not included.
Shade Trees	193	ea	\$500	\$96,500	Along pathway assume 60' o.c.
Bike Bridge	1	ea	\$100,000	\$100,000	Prefab. construction
Landscape Allowance	1	ls	\$20,000	\$20,000	
Site Amenities	1	ls	\$30,000	\$30,000	benches, trash receptacles, bike racks
Amenities Subtotal				\$736,500	
35% Contingency				\$257,775	Engineering, Design & Unknowns
<b>BUSINESS PARK AMENITIES TOTAL</b>				<b>\$994,275</b>	



**RESOLUTION NO.**  
**TOWN OF LEDGEVIEW TOWN BOARD**

**WHEREAS**, the Town of Ledgeview continually works to guide and coordinate decisions and development within the Town; and

**WHEREAS**, the Town of Ledgeview has hired Schreiber/Anderson Associates to plan a business park near Interstate 43 and CTH MM, the Town's only interstate interchange; and

**WHEREAS**, several public meetings were held to obtain public input during the development of the business park master plan and these meetings included stakeholder interviews in December 2005 and public meetings on December 12, 2005, February 9, 2006 and April 20, 2006;

**NOW, THEREFORE BE IT RESOLVED** that the Town Board of the Town of Ledgeview, Wisconsin, does by enactment of this resolution formally adopt the document called the "Ledgeview Business Park Master Plan – Development Strategy and Action Plan".

Approved this \_\_\_\_ day of \_\_\_\_\_, 2006.

\_\_\_\_\_  
Jeffrey J. Van Stratent, Chairman  
Town of Ledgeview

ATTEST:

I, Sarah K. Burdette, Clerk/Administrator, of the Town of Ledgeview, Brown County, hereby certify that the above is a true copy of a resolution adopted by the Town of Ledgeview on the \_\_\_\_ day of \_\_\_\_\_, 2006

\_\_\_\_\_  
Sarah K. Burdette, Clerk/Administrator  
Town of Ledgeview

Posted: \_\_\_\_\_